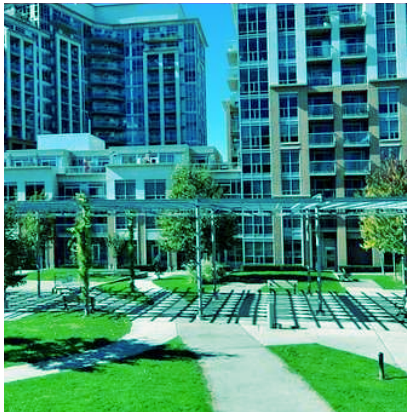




Northwest Yonge & Green Lane

public realm design & urban
neighbourhood structure, form
and design report

February 2025





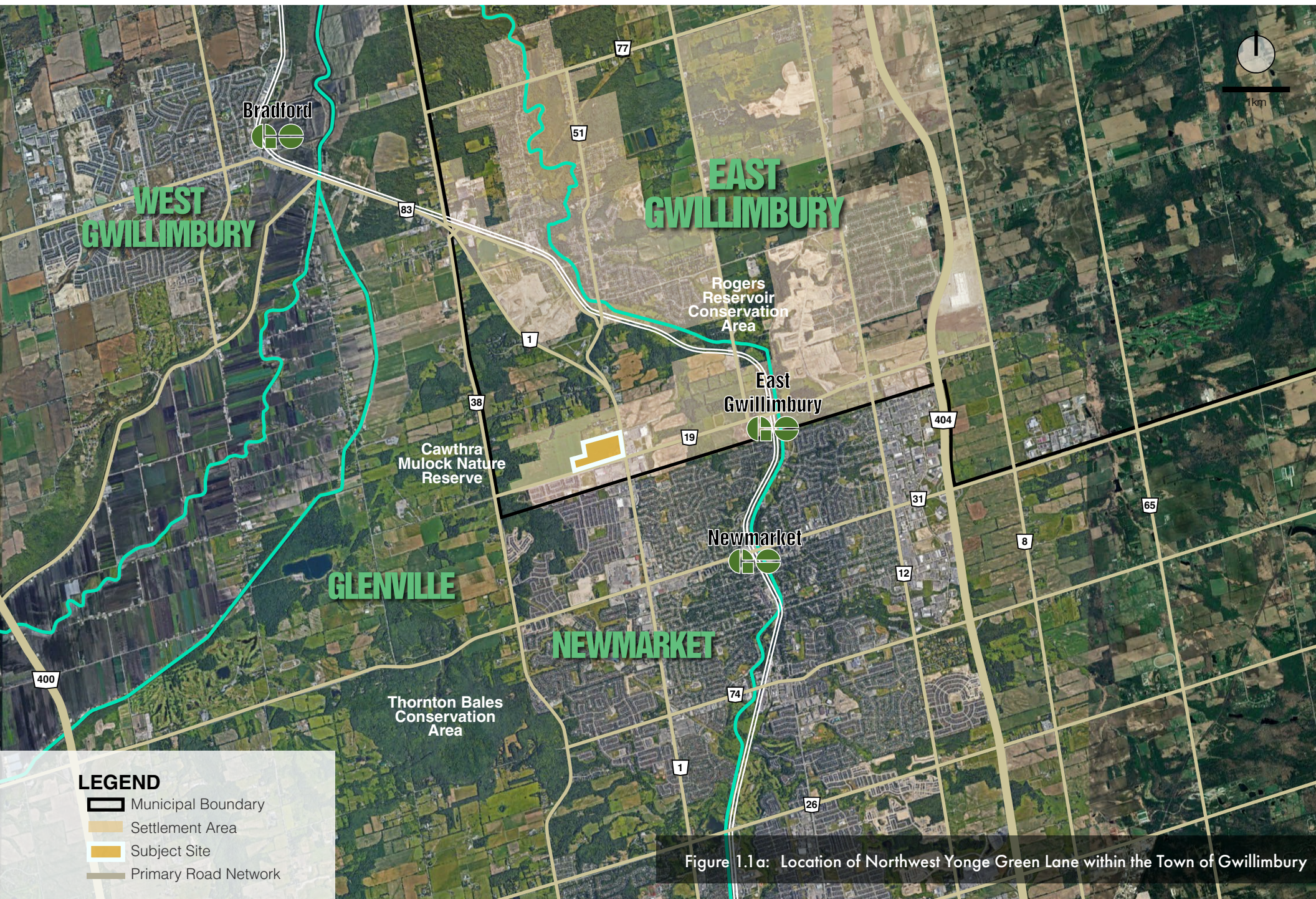
First Submission

Prepared For
Northwest Yonge and Green Lane Landowners Group Inc.

by
NAK 
design strategies

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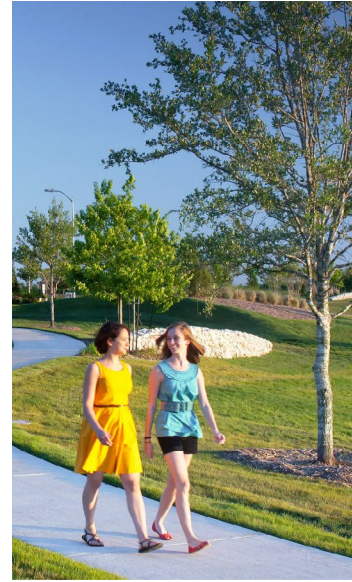
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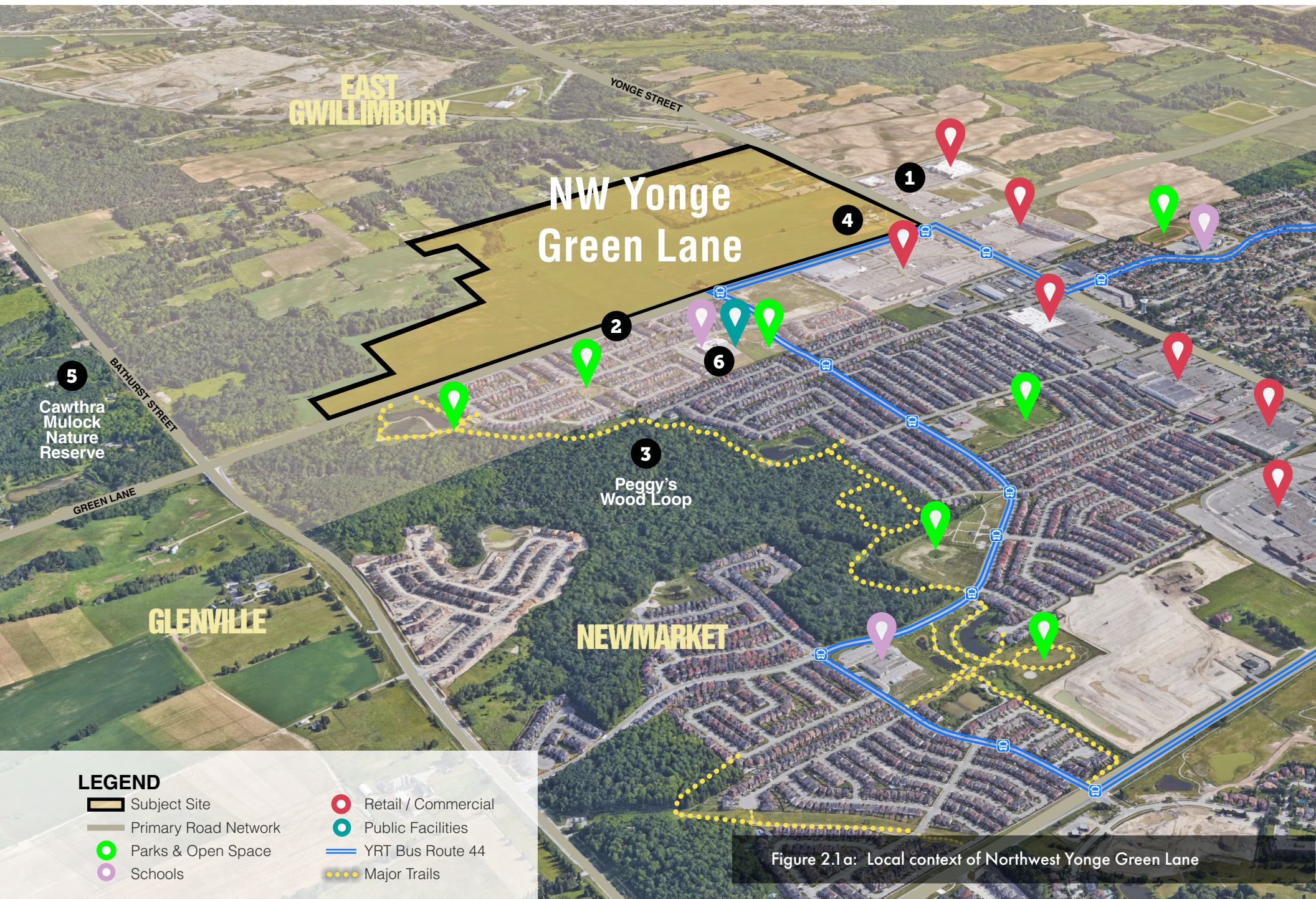
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1.1 Purpose of the Document

This document is intended to provide guidelines for *Public Realm Design and Urban Neighbourhood Structure, Form and Design* as part of a Community Design Plan (CDP) for the lands owned by the Northwest Yonge and Green Lane Landowners Group Ltd. Located within the Town of East Gwillimbury in the Region of York, north of the Town of Newmarket, and west of the Township of Whitchurch-Stouffville, the lands form part of the Green Lane Secondary Plan as designated by Official Plan Amendment No. 4-2018.

In support of the CDP, the *Public Realm Design and Urban Neighbourhood Structure, Form and Design* will address the policies of the York Region Official Plan, the East Gwillimbury Official Plan, and the Green Lane Secondary Plan, outlining the physical design of the community's public realm, with particular focus on streetscape, parks, and open spaces. It will also provide design direction for the community's built form and design, while allowing some flexibility for delivering a wide range of design expressions, and architectural form and styles that provide interest in the urban environment. Expanding on the 2016 Urban Design Guidelines (UDG) that are currently in-effect, this document provides an update based on the revised CDP with additional details, guidelines, and design direction.





site context

2

Northwest Yonge Green Lane (herein known as NW Yonge Green Lane or the 'subject lands') is an approximately 115 hectare (284 acre) proposed development located at the north-west corner of Yonge Street and Green Lane West. The CDP proposes a mix of low-high density uses that is intended to reflect the Regional and Town vision for the Green Lane Secondary Plan Area.

2.1 Site Location

NW Yonge Green Lane's location at the intersection of major arterial roads offers an opportunity to create a complete community with strategic density where the Town anticipates major growth. The new community will be bounded -

- To the north by the proposed Major Collector Road. North of the Major Collector Road is outside of the Town's Settlement Area Boundary.
- To the east by Yonge Street, a Regional Arterial road. An existing low-rise retail/commercial node is located on the east side.
- To the south by Green Lane West and the Town of Newmarket's northern boundary. An existing low-rise retail plaza is located on the south side of this Regional Arterial Road. Further south east is low density residential and a nature trail, Peggy's Wood Loop.
- To the west by the Cawthra Mulock Nature Reserve situated within the Greenbelt system.



Commercial node on the east side of Yonge Street



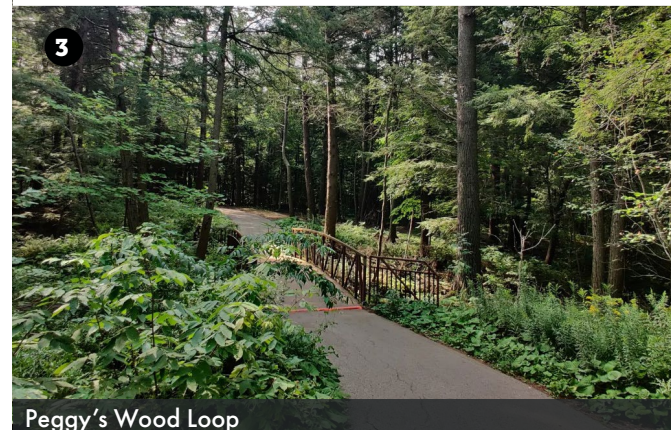
Existing rural uses on the subject lands



Low density residential south of Green Lane West



Cawthra Mulock Nature Reserve



Peggy's Wood Loop



Harvest Hills Activity Centre



Figure 3.1 a: Communities in York Region with a protected Greenbelt System

policy context

3

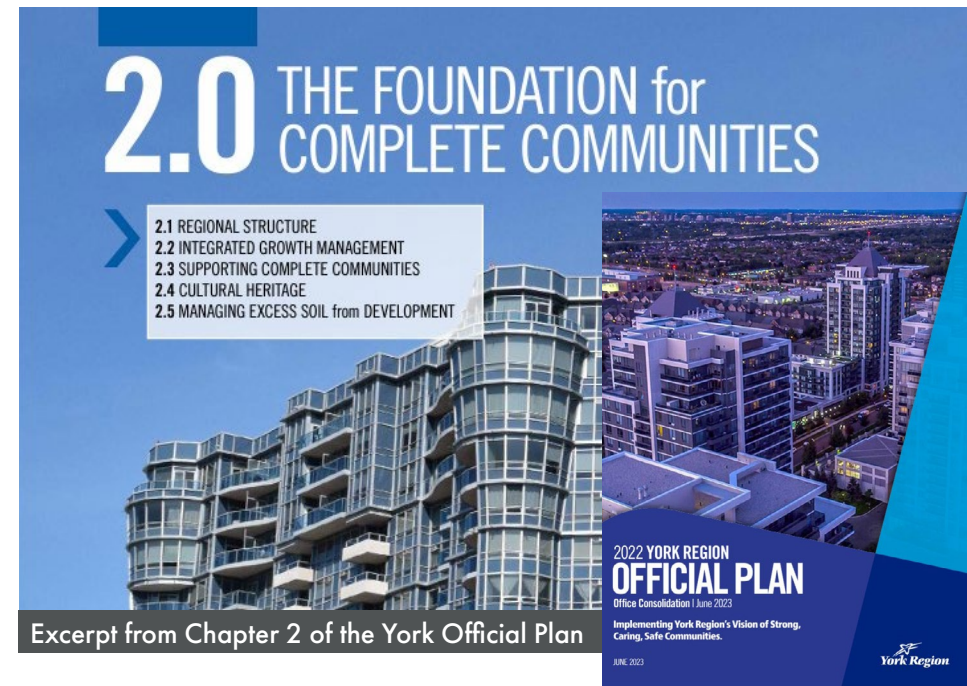
The proposed development for NW Yonge Green Lane is subject to several planning studies and processes. This document outlines a design strategy consistent with the objectives of the following documents:

3.1 York Region Official Plan

As one of Ontario's fastest growing municipalities, the 2022 Regional Official Plan describes how to accommodate future growth and development while meeting the needs of existing residents and businesses in the Region. It provides policies for economic, environmental and community planning decisions that will be integrated into Local Municipal Official Plans.

The Plan identifies major goals that provide overall high-level direction for specific objectives and policies contained in each Chapter. Designed as a 'complete community' that offers a range of housing opportunities, the CDP for NW Yonge Green Lane aligns with the Region's goals, including:

1. To enhance York Region's urban structure through a comprehensive integrated growth management process that provides for healthy, sustainable, complete communities with a strong economic base.
2. To protect and enhance the natural environment for current and future generations so that it will sustain life, maintain health and provide a high quality of life.
3. To enhance York Region's urban system through city building, intensification, and compact and complete communities including employment areas.



4. To protect the Agricultural, Rural and Holland Marsh Specialty Crop Areas and support the agricultural industry as essential components of the Regional fabric.
5. To provide the services required to support York Region's residents and businesses to 2051 and beyond, in a financially and environmentally sustainable manner.
6. To ensure resiliency and the ability to adapt to changing economic and environmental conditions and increasing social diversity.

3.2 Town of East Gwillimbury Official Plan

The East Gwillimbury Official Plan (OP) sets the vision for the long term growth and development within the Town over the planning period to 2031. Prepared with input from members of the public, stakeholders, Town Staff and Council, the OP provides policy direction for the allocation of land uses, the long term goals for services and infrastructure, and protection of the Town's natural environment.

The CDP for NW Yonge Green Lane reflects the overall guiding vision for the Town to 'develop a complete, healthy and sustainable community that will provide opportunities for residents in all life stages to live, work, play and learn.' Refer to the UDG for a comprehensive outline of all relevant Town policies to NW Yonge Green Lane.

Consistent with the policy framework provided in the Town's OP, the following key policies apply to the subject lands:

3.2 Urban Area Structure

3.2.3.2.1 Major Local Centres

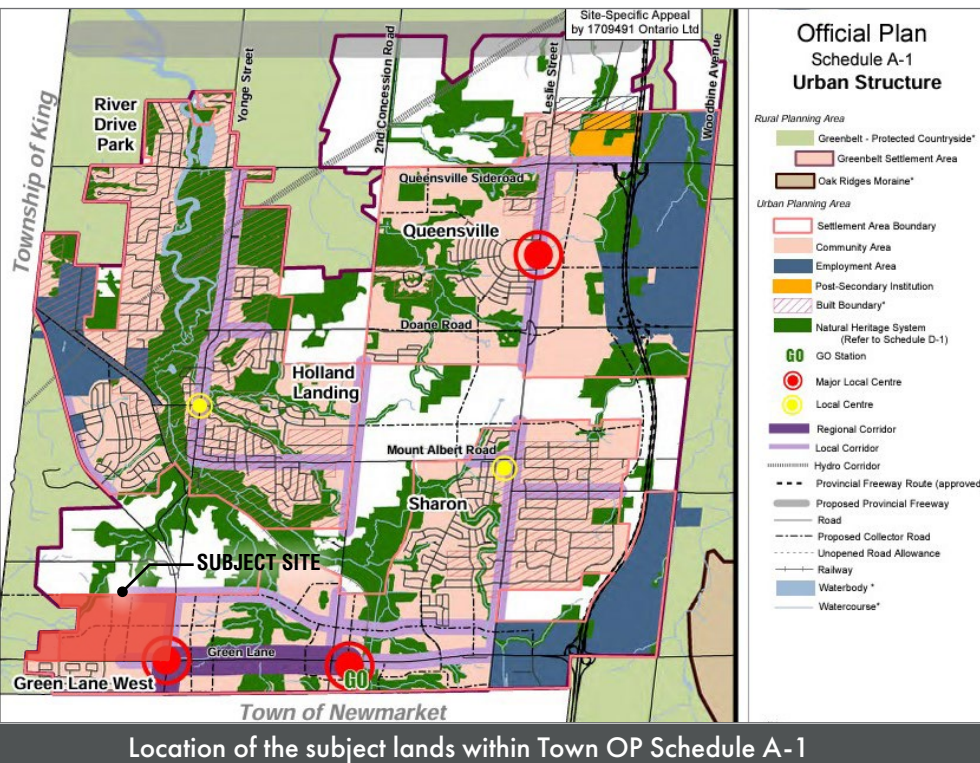
The Yonge Street and Green Lane Centre is identified as one of the three Major Local Centres, as indicated on Schedules A-1 and B-5. Major Local Centres are generally intended to be a primary gathering place that incorporates the highest densities of housing and concentrations of population-related employment that serve the larger community. This Major Local Centre is intended to have a 'major focus of region-serving commercial development that is planned to intensify with a greater mixture of residential and population-related employment uses to promote increased pedestrian and transit-orientation.' The lands within this designated area shall align with the specific policies for uses, building locations, pedestrian oriented retail priority areas, affordability, public art, and phasing.

3.2.3.3 Corridors

As identified on Schedule A-1, 'Local Corridors' are identified within the subject lands, framing NW Yonge Green Lane on the east, northeast and southeast. The CDP is consistent with the policy that corridors shall be designed to support an urban, mixed-use character that is pedestrian-oriented and supports transit use.

3.5 Intensification

Aligning with Schedule A - Town Structure, the subject lands are designated as 'Community Area' with a 'Major Local Centre' at Yonge Street and Green Lane. As described in the OP, Policy 3.5.1 states that 'Growth through intensification in Community Areas shall provide a diverse and compatible mix of land uses, including residential and population-related employment uses.'



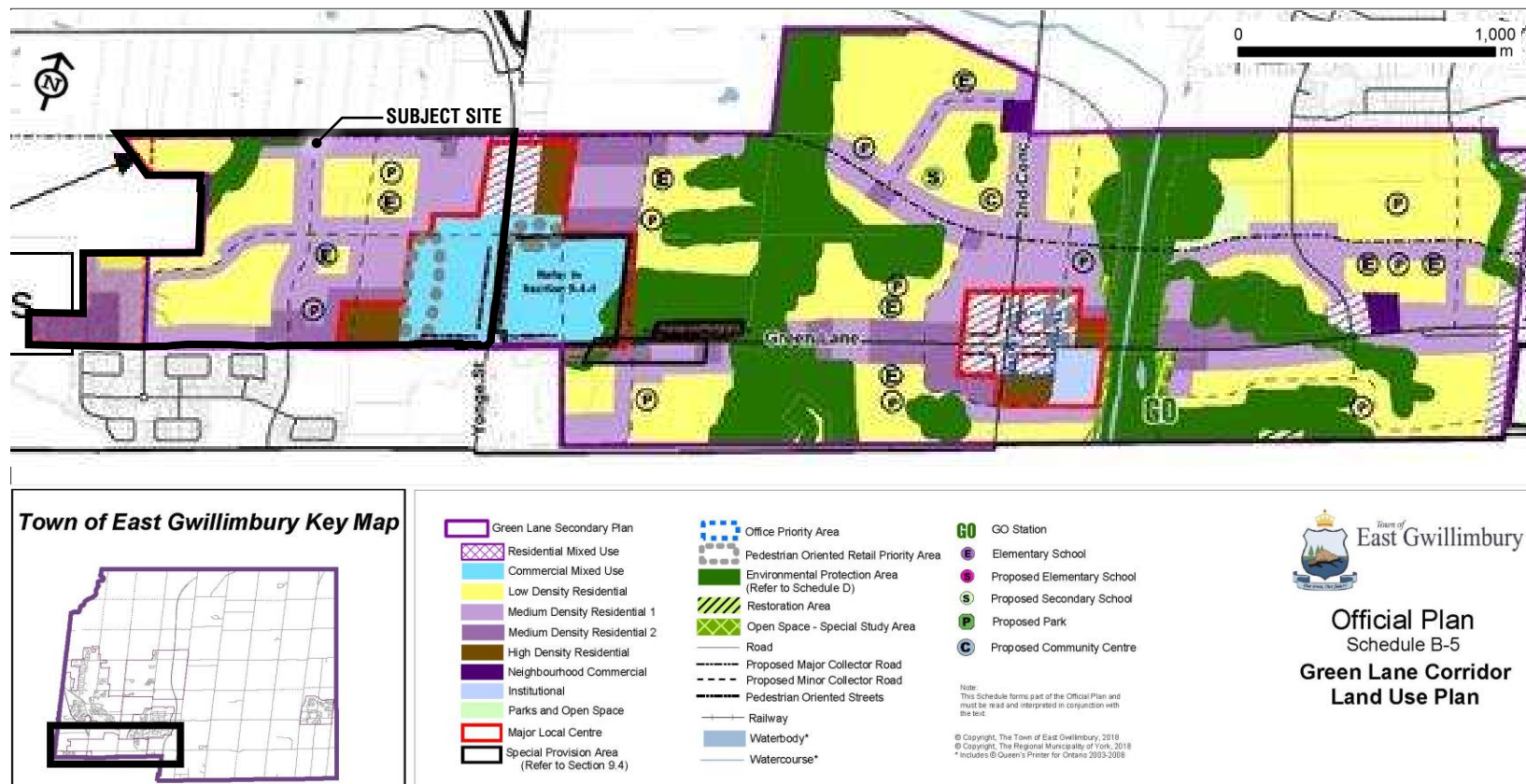
3.3 Green Lane Secondary Plan

A Secondary Plan for the Green Lane Planning Area provides another layer of the Town's visioning document that helps to further define, guide, and implement the development for these lands, guiding the overall physical development.

The NW Yonge Green Lane CDP generally reflects the structure and land use distribution designated in the Secondary Plan, including:

- Commercial Mixed Use located along the Yonge Street and Green Lane West corner where there are higher intensity uses anticipated in the Major Local Centre;

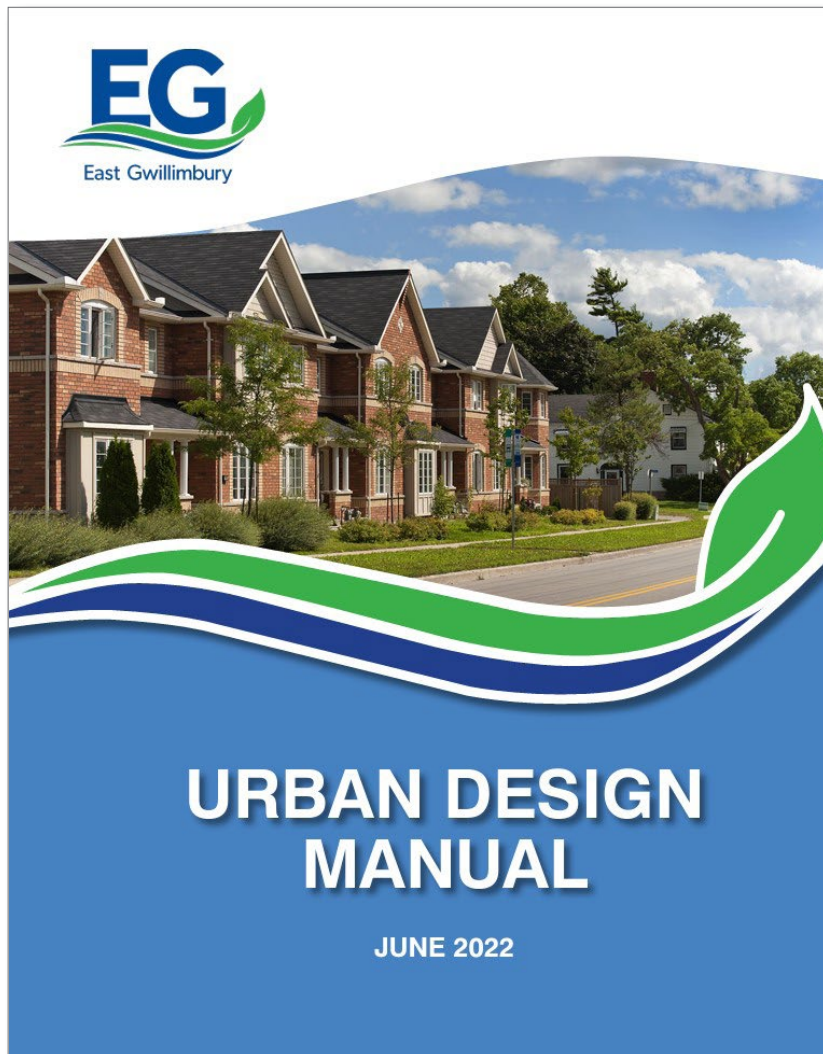
- A mix of Medium Density Residential Uses focused along Major and Minor Collector Streets;
- High Density residential located along Green Lane West in the Major Local Centre;
- Two (2) Elementary Schools, proposed public parks, and several privately owned public spaces within development blocks;
- Low Density Residential in the central and western portion of the subject lands;
- Greenbelt System / Preserved Environmental Protection Area.



Schedule "BB" - Green Lane Land Use Plan, OP Amendment No. 4-2018

3.3 Town of East Gwillimbury Urban Design Manual (UDM)

The Town's Urban Design Manual establishes a Town-wide standard to promote an attractive, high quality and sustainable community in East Gwillimbury. The Manual provides the overall vision, design principles and objectives for the Town across the private and public realm.



The provisions, and examples in the UDM should be used as the foundation of design for all development projects in the Town and will be used in the assessment of development proposals. This manual intended to be read in conjunction with, and complement the objectives and policies of the Official Plan, the provisions of the Town of East Gwillimbury Zoning By-law, Thinking Green Development Standards, master plans, secondary plans, community design plans, and other guidelines or standards. The UDM shall apply to all projects subject to review and Planning approval by the Town through subdivisions, condominiums, and site plan control applications as permitted under the Planning Act and the Zoning By-law.

General urban guidelines are provided in the UDM and shall apply to NW Yonge Green Lane:

1. *Encourage opportunities for vibrant, diverse and pedestrian-oriented urban environments that provide for public safety, changing experiences, social engagement, meaningful destinations. and*
2. *Provide for mixed-use neighbourhoods that are walkable with connected public gathering places, where opportunities for social interaction are increased and services can be provided within easy walking or cycling distance or by use of public transit.*
3. *Support the 15-minute community by ensuring a typical walking distance to a variety of services and facilities, such as of 400 metres (5 minute walk) to daily activities, such as transit (local bus routes), elementary schools, active parks, and modest services, or 800 to 1,200 metres (10 to 15 minute walk) to secondary schools, higher order transit, or Centres and Corridors, among others.*
4. *All modes of mobility and users should be given equal consideration in the planning, design, and construction of new developments to achieve a balanced, pedestrian-oriented approach.*



Figure 3.3a: A recently constructed park in the Town of East Gwillimbury

5. *Promote internal connectivity and multiple connections to the community at large, taking into account the existing and proposed urban structure of adjacent and adjoining areas.*
6. *To ensure the effective continuity of the street pattern and implementation of long range active transportation plans:*
 - a. *Gaps in the existing street grid shall be completed by providing connecting streets through developments;*
 - b. *Adjoining streets shall be extended into developments and subdivisions; and,*
 - c. *Streets shall be extended to the boundaries of the development as appropriate to accommodate further extension of the street pattern and to create interconnections to adjacent neighbourhoods and uses.*
7. *Provide for an interconnected network of sidewalks, bicycle routes, transit, and multi-use trails ensuring proper integration with surrounding neighbourhoods and a variety of destinations, allowing for continuous movement throughout the Town.*
8. *Locate and design parks and open spaces to support, complement, and buffer the Natural Heritage System.*

3.4 Town of East Gwillimbury Parks Design Manual (PSDM)

With a purpose to set community standards for all outdoor leisure amenities, the Town's Parks Design Standards Manual (PDSM) provides guidelines to design and maintain high-quality parklands and recreational facilities. As a resource for staff, Council, and development partners, the manual also provides expectations for parkland conveyance and development for the community and its stakeholders.

The PSDM is organized in three sections:

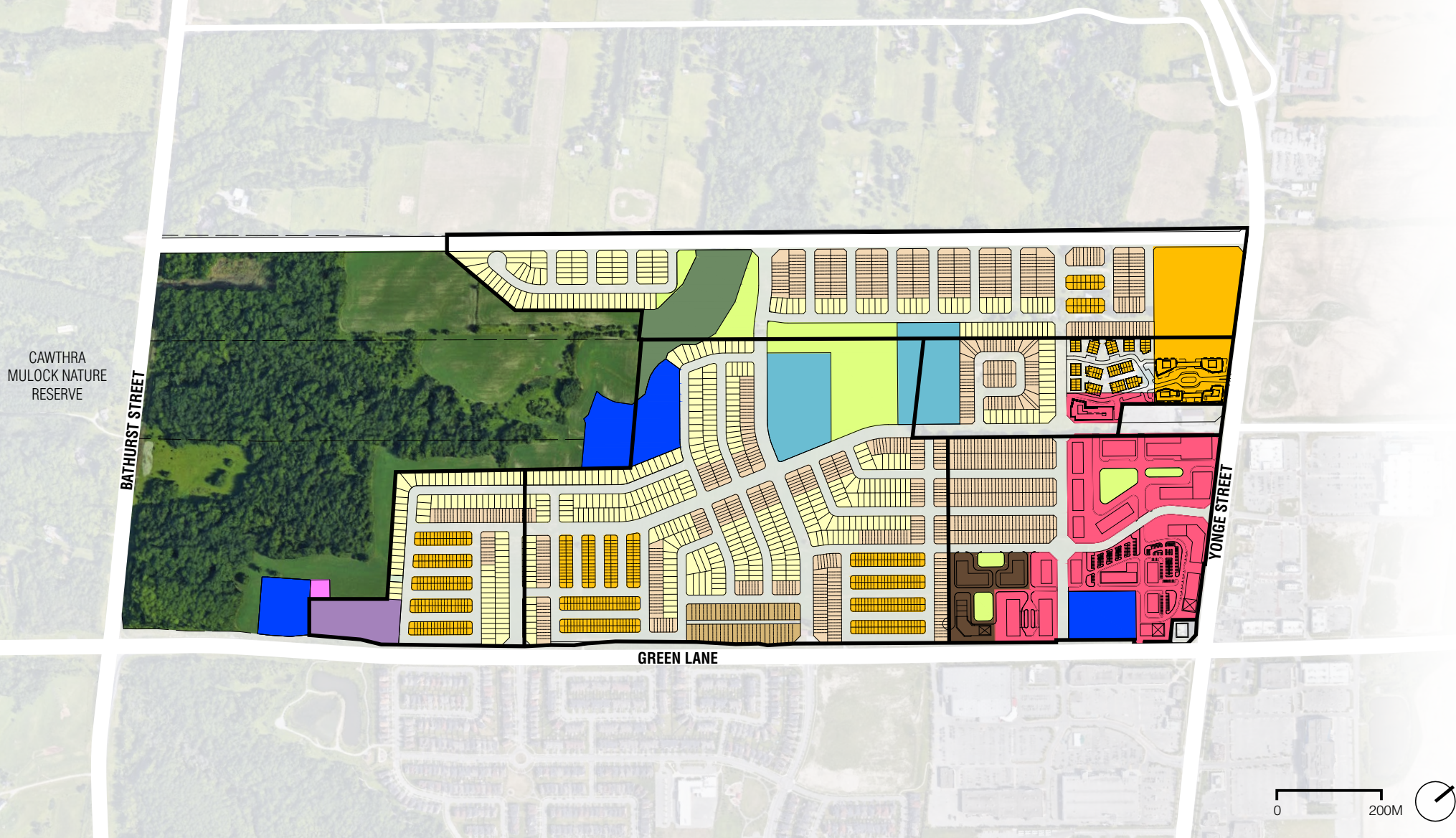
Section 1: Community Planning Context and General Guidelines

Section 2: Park Design and Development

Section 3: Park Facilities and Construction Standards Detail

The following guidelines in the manual specifically apply to the parkland facilities in NW Yonge Green Lane:

- Outdoor athletic fields
- Parks and trails



LEGEND

Subject Site Boundary	Back-to-Back Townhouse	Commercial Mixed Use	Pump Station
Single Detached	Medium Density Residential	High Density Residential	Environmental Protection Area
Street Townhouse	Residential Mixed Use	Park/Amenity	Stormwater Management Facility
Rear Lane Townhouse		Institutional	Servicing Block

Figure 4.0a: NW Yonge Green Lane Community Design Plan

community design plan

4

The overall layout and distribution of uses within NW Yonge Green Lane have been designed to maximize the use of land and to support the overall objective of an accessible and pedestrian oriented community. The design of this community is based on an efficient street network, an integrated parkland and open space system, and a well-defined mixed-use area to serve the community and beyond.

As described in the UDG, the urban design objectives for the development of the Green Lane Secondary Plan are as follows:

- *Development of a compact urban form that promotes a vibrant sense of community, and encourages pedestrian and other alternative modes of movement while creating residential densities that are supportive of transit.*
- *Development of a hierarchy of streets that provide for the efficient movement of vehicular traffic and also contribute to the public realm through a land use pattern that encourages walking, creates pedestrian oriented development and streetscapes, links the components of the community and provides the infrastructure for alternative modes of transportation.*
- *Promotion of connectivity within and to areas adjacent to the Secondary Plan Area, encouraging pedestrian as well as vehicular movement, building on the central spines running both north-south and east-west.*
- *Appropriate placement of public walkways that further connectivity.*
- *Provide a diverse and high quality commercial mixed-use area*

which fosters a pedestrian promenade through the placement of low-rise commercial buildings and a continuous building façade edge that frames the street and facilitates a strong pedestrian environment.

- *Provision of community focal points through the location of gathering places along the central spines and locations within the mixed-use area.*
- *Reinforcement of the importance of public uses within the Secondary Plan Area to enhance their role through design, location and orientation.*
- *Promotion of safety and security for all persons.*
- *Design of service and parking facilities to minimize the disruption to the safety of pedestrian movement and the attractiveness of development adjacent to the public realm.*
- *Reinforcement of the relationship of the buildings to the street by avoiding reverse residential lotting adjacent to streets.*
- *Promotion of the relationship of buildings to streets through the arrangement of buildings on lots, setbacks to the street, the placement of parking and garages, and the use of specific architectural treatment where the side façade of a building abuts a street, with a view to creating street presence.*

Based on these broad objectives and the guidelines provided in the UDG, the following sections provides further design direction for the public realm, urban neighbourhood structure, and built form.



“ The vision is develop a complete, healthy and sustainable community that will provide opportunities for residents all life stages to live, work, play and learn. ”

Town of East Gwillimbury Official Plan Section 1.3

Figure 5.0a: Example rendering of a high quality pedestrian environment

public realm design

5

NW Yonge Green Lane's public realm will be designed to reinforce a compact, pedestrian scaled urban village environment that prioritizes safe and convenient walking and cycling connections, and access to transit. With a Rapid Transit Corridor designated on Yonge Street south of Green Lane, and along Green Lane East to the East Gwillimbury GO Station, there is an opportunity to establish transit-supportive densities along with active transportation connections that will foster a healthy, sustainable community. The treatment of the public realm should reflect high quality pedestrian environments, with coordinated landscape features, built form, infrastructure, and utilities.

In addition to the UDM guidelines and Town OP policies for this Major Local Centre, the following guidelines will discuss the proposed elements within the public realm and provide design considerations for the new community.

5.1 Streetscape

The streetscape will play an important role in establishing the character of NW Yonge Green Lane and creating a pedestrian friendly environment. Streetscape design involves several components, including street trees, site furniture, lighting, utilities, gateways, fencing and signage. All streets within the new community are intended to provide a comfortable pedestrian experience. The proposed streetscape treatment shall be appropriate to the street designation as established through the road hierarchy presented in Section 5.2 of this document.

In addition to the Town OP policies outlined in section 3.3.1 *Public Realm*, the following general streetscape guidelines shall apply to all proposed road types.

5.1.1 Street Trees

Proposed planting for NW Yonge Green Lane shall achieve a balance between enhancing the vegetated environment through ecological sustainability and urban tree canopy, and meeting aesthetic requirements.

Healthy street trees reduce air pollution, provide shade and cooling, furnish habitat for wildlife, increase property values, enhance community aesthetics and pride of place, make streets safer and more walkable, and contribute to quality of life. An effective planting strategy can help establish or reinforce the character of the community. As per OP Policy 3.3.1.5, *'Street tree planting shall be included in the design of all streetscapes to contribute to a high quality of landscaping and promote pedestrian comfort. Tree planting should be continuous and the species, placement and frequency of street trees should be appropriate for the type of street.'* The following tree planting guidelines may be considered:

- Tree planting shall comprise hardy species tolerant of urban conditions (pollution/salt/drought tolerant, compacted soils);
- Generally, preference shall be given to native species;
- Selection of proposed tree species and caliper size shall be from the Town's recommended list;



Figure 5.1a: Coordinated Site Furniture in key open spaces areas.

- To foster greater biodiversity, street tree mono cultures that repeat the same species over large areas shall be avoided;
- Planting conditions inherent in many urban environments, which are characterized by minimal soil volumes, poor soil structure, lack of irrigation, and improper drainage, shall be avoided; and
- Good quality soil shall be retained on site and enhanced with locally sourced soil, when possible, of equal or better quality.

Particularly, within the Major Local Centre there is a great opportunity to elevate the urban street experience through tree planting, which may include:

- Urban street tree treatments that utilize soil cells to provide ample soil volume below paved and walkable boulevards, anticipating a higher volume of pedestrian traffic and the encouragement of mid-block crossing patterns. This ensures long term healthy tree growth, a robust street tree canopy, and also provides opportunities for front store patios that can animate the hub with outdoor extensions of cafes and restaurants; and
- Ornamental planting opportunities, including raised beds and/or pots, assumed to be maintained by the private condominium ownership, which can bring colour and seasonality to the public realm.

5.1.2 Site Furniture

Attractive, sturdy, and accessible street furniture is fundamental to the visual appeal and use of streets and public spaces. It plays an important role in defining the streetscape and reinforces the community identity.

Design Guidelines:

- Street furniture shall be provided in high pedestrian traffic areas including the Major Local Centre, and within key open space areas, such as parks and stormwater management facility lookouts;

- The colour, material, form, and style of street furniture shall be consistent with and complementary to the established design theme for the community;
- The placement and layout of furnishings shall encourage safe use, maintain all accessibility requirements and be appropriate to the adjacent built form type and function;
- As much as possible, furnishings shall be vandal-resistant and low-maintenance, with readily available componentry; and
- Furniture within the school /park blocks, in particular, shall include benches, waste receptacles and bicycle racks, rings or posts, and shall be complementary to the selected street lighting design.

5.1.3 Street Lighting & Utilities

Proper lighting design is critical to ensuring safe pedestrian and vehicular circulation. Lighting design (pole and luminaire) shall be consistent with the surrounding neighbourhood, in an architectural style that reinforces a definable character for the community. Street light poles and luminaires shall reflect approved Town standards, complementary to the surrounding neighbourhoods.

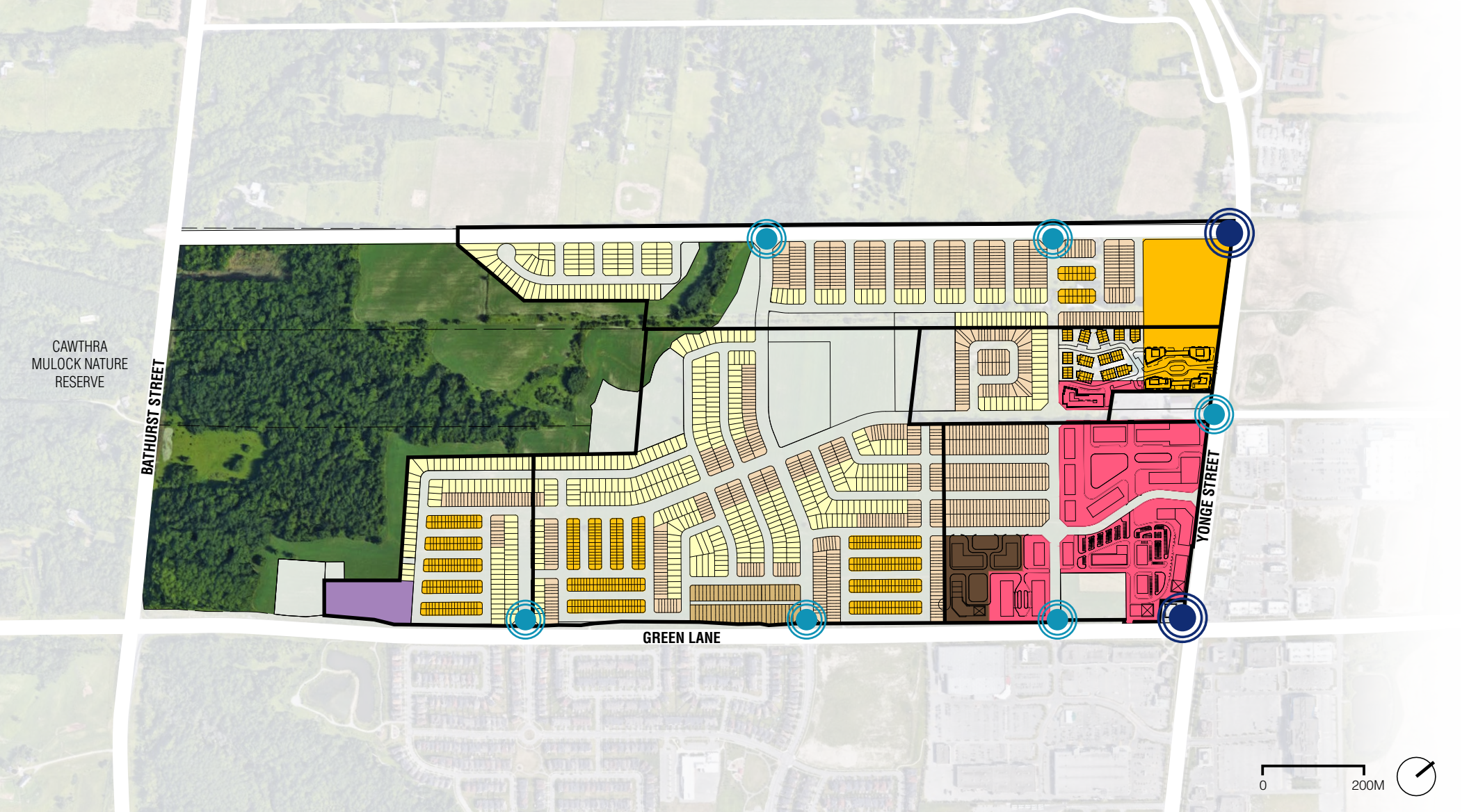
The Major Local Centre may be distinguished by a special lighting treatment to reinforce its role as a key character area for the community. This may include considerations for pedestrian scale light standards that further reinforce character areas and their intended use throughout the day and evening.



Figure 5.1b: Pedestrian scale light standards reinforce character areas.



Figure 5.1c: Pedestrian crosswalks provide visual cues for slowing traffic speeds.



LEGEND

Subject Site Boundary	Back-to-Back Townhouse	Commercial Mixed Use
Single Detached	Medium Density Residential	High Density Residential
Street Townhouse	Residential Mixed Use	Primary Gateway
Rear Lane Townhouse		Potential Secondary Gateway

Figure 5.1d: NW Yonge Green Lane Proposed Gateway Locations

5.1.4 Gateways & Entrance Features

Appropriately designed gateway features provide a sense of identity, signal arrival, serve as placemaking and wayfinding elements, and enhance the visual quality of the public street. Gateways are an effective tool for branding the development through the integration of signage. Together with the adjacent built form, they help define the character of the development from the site surroundings.

Within NW Yonge Green Lane, the intersection at Yonge Street and the Major Collector will serve as a primary gateway, with potential secondary gateways linking the community to the north, east and south, where appropriate. The north-east corner of Green Lane West and Yonge Street will also serve as a primary gateway. At the north, the intersections of the proposed Minor Collectors with the Major Collector may serve as secondary north-south gateways. Similarly, along the south, the intersection of the Minor Collectors with Green Lane West may also provide secondary gateways. At the east, the intersection of Yonge Street and the Minor Collector will provide the east-west secondary gateway.

Gateway features may comprise landscaped and/or architectural features, such as prominent built form that addresses the street corner, reinforced by landscape elements within the private lands. In addition to Town OP Section 3.3.2 *Gateways, Edges and Landmarks*, and the guidelines provided in the section 5.6 *Community Gateways* and 5.7 *Community Edges* of the UDG, the following design guidelines may be considered for gateways and entrance features:

Design Guidelines:

- Gateway features on private lands may incorporate enhanced architecture and both hard and soft landscape elements with consideration for low walls, columns, signage, landscape lighting, enhanced paving, and ornamental planting;
- Buildings shall be designed with active façades and prominent built form, and located to frame the gateway and reinforce a sense of entry into intensification areas;
- Gateways at the intersection of Regional Roads should be coordinated with the Region of York, and utilize the Region of York Municipal Streetscape Partnership Policy to leverage capital improvements;
- Consistency and coordination of materials, colours, forms, and elements shall be provided for the landscape components;
- The design of landscape elements shall be coordinated with the adjacent built form, reinforcing the prominent architectural features;
- Signage design shall be consistent with the proposed architectural theme; and
- The design and layout of gateways shall not impede required view angles.



Figure 5.1e: A privacy fence at residential flankage locations.



Figure 5.1f: Chainlink fence for lots adjacent to open space features.

5.1.5 Fencing

Fencing of varying types and materials will be required throughout the community to address barrier, privacy and acoustic requirements. In areas of high visibility, fencing shall be designed to enhance the streetscape appearance, with consideration for long-term maintenance requirements. Locations for integrating fencing will be determined in the detailed design stage and may include:

- Privacy fence at residential flankage locations;
- Low decorative fencing (metal or wood) at gateway entries, and along window streets; and
- 1.8m high chain-link fencing for lots adjacent to public open space features and schools.

Design Guidelines:

- Fencing design shall be coordinated and consistent throughout the community;
- Fencing design shall reinforce or complement the character and identity of the community;
- Fencing shall comprise only robust, sturdy components for long term durability; and
- Intricate design work using smaller components should be avoided for wood fencing due to the effects of weather over the long term.

5.1.7 Public Art

As per Town OP Policy 3.3.2.6, it is recommended that public art is incorporated into the design of urban squares, parks and pedestrian spaces at primary gateways and encouraged as a method of reinforcing the focal nature of spaces. Additionally, the *'inclusion of public art is encouraged in all significant private sector developments in the Yonge Street and Green Lane Major Local Centre, and 1% of the capital budget of all major Regional and Local municipal buildings in this Centre is to be dedicated to public art.'*

5.1.6 Commercial Signage

The following design guidelines apply to the commercial blocks where signage may be considered for both street fronting façades and units fronting internal private roads and parking lots:

- Given that some commercial entries will be from the internal parking lot side, signage along the street elevation may be less prominent than that facing the parking lot;
- Regardless of street or parking elevation and hierarchy, the design of individual unit signage shall be visually and thematically consistent with the building design and coordinated throughout the site;
- Consistency of the design approach to signage should be encouraged;
- Signage should be considered secondary to the architectural design and massing of the building;
- Signage shall be visible from both vehicles on the road and pedestrians on the sidewalk;
- Cut-out letter signage and corporate logos shall be encouraged to complement the type of use and should be compatible in size, style, colour, shape and material;
- Signage shall be lit by fixtures that are consistent with the architectural style;
- The design of retail signage at the corner shall be visually and thematically consistent with the building design and coordinated throughout the site; and
- Ground related signage structures should be coordinated with the landscape design.



Figure 5.1g: A pedestrian crosswalk enhanced with decorative unit paving.



Figure 5.1h: Distinctive paving treatments in a mixed-use area.

5.1.8 Traffic Calming and Pedestrian Crosswalks

Traffic calming is key to promoting walkability and creating a safe, pedestrian-friendly environment. Pedestrian crosswalks serve to demarcate a safe route for pedestrians to cross the street, thereby delineating a separation between the pedestrian realm and vehicular zones and encouraging traffic calming. Providing visual cues for slowing traffic speeds, encourages cautious driving.

Although vehicles will continue to move efficiently through the community, particularly along the minor collector streets, traffic calming measures derived from the form and minimized widths of the street and its component parts will also reinforce slower speeds. These calming measures will bring an identifiable character to the public realm, where building face-to-building face distances are minimized and effectively frame the rights-of-way with a strong built form relationship between at-grade uses and the street.

Additionally, street crossing distances are shortened, vehicular travel lane widths and corner radii are minimized and, within the Major Local Centre, upgraded paving treatments and furniture will reinforce the district as a pedestrian priority zone. The following design principles should be considered in the design of traffic calming and pedestrian crosswalks:

- In high pedestrian traffic areas a formal pedestrian crosswalk installation should be provided at four-way intersections;
- To improve visibility and reduce conflicts between pedestrians and motorists, crosswalks at crucial intersections should incorporate distinctive coloured and/or textured materials or markings. Additionally, these elements may be integrated with any decorative paving, such as unit paving, that is complementary to or a continuation of the enhanced paving treatment within the district; and
- To assist pedestrians with visual impairments, curb ramp designs shall have raised tactile surfaces or materials with contrasting texture and sound properties.

5.1.9 Community Safety

Ensuring a safe, comfortable environment for all residents, employees, and visitors of NW Yonge Green Lane, both during the day and at night is a critical element to responsible built form and open space design. Consideration shall be given to the design and siting of all buildings that incorporate the principles of CPTED (Crime Prevention Through Environmental Design).

Design Guidelines:

- All publicly accessible areas, both interior and exterior, should be well lit throughout the day and evening;
- Gateway features or landscape buffers should not obstruct views at critical junctions involving vehicles, pedestrians and cyclists (i.e. at intersections, gateways and driveways);
- Views from buildings, particularly ground floor uses, should be provided towards publicly accessible outdoor areas;
- Outdoor amenity spaces should be situated within easy visibility from adjacent buildings;
- Building walls should be designed as clear and clean, as appropriate, without nooks or alcoves that may provide hiding spots; and
- Alternative or emergency exits from buildings or underground parking should connect with highly visible areas.



Figure 5.1i: A safe and pedestrian-friendly space may be designed through the placement of buildings and landscaping.



Figure 5.1j: Street trees and soft landscaping provide shade and screening for parking and servicing areas.

5.1.10 Parking and Service Areas

NW Yonge Green Lane aims to cater to diverse needs of its residents and visitors, with strategically placed parking areas for easy accessibility, on-street parking to enhance the urban experience, and discreetly located loading facilities.

The following design guidelines will help promote safety, accessibility, and environmental consciousness, whilst creating a vibrant and functional community:

- Ensure that parking and service areas are conveniently located and easily accessible from major entry points, public transportation, and key amenities. Properly zone the areas to allocate spaces for different user groups, including residents, visitors, employees, and service vehicles;
- Implement adequate lighting and surveillance to enhance safety and security in the parking and service areas. Install emergency call stations and prominently display clear signage to direct users effectively;
- Integrate green spaces and landscaping elements to enhance the aesthetics of the parking and service areas;
- Where possible, utilize sustainable materials and stormwater management techniques to promote environmental responsibility; and
- Ensure the design of parking and service areas is inclusive and accessible to people of all ages and abilities.

5.1.11 Accessibility

Barrier-free accessibility is an integral part of the NW Yonge Green Lane community and site design considerations in an effort to eliminate barriers within the built environment so that people with physical disabilities will have more opportunities to participate in everyday life. Designs shall be proposed that are in accordance with the Accessibility for Ontarians with Disabilities Act (AODA) and any other applicable legislation, including York Region's Accessibility Policy, and the Town's Accessibility Policy #10-300-CP-056.

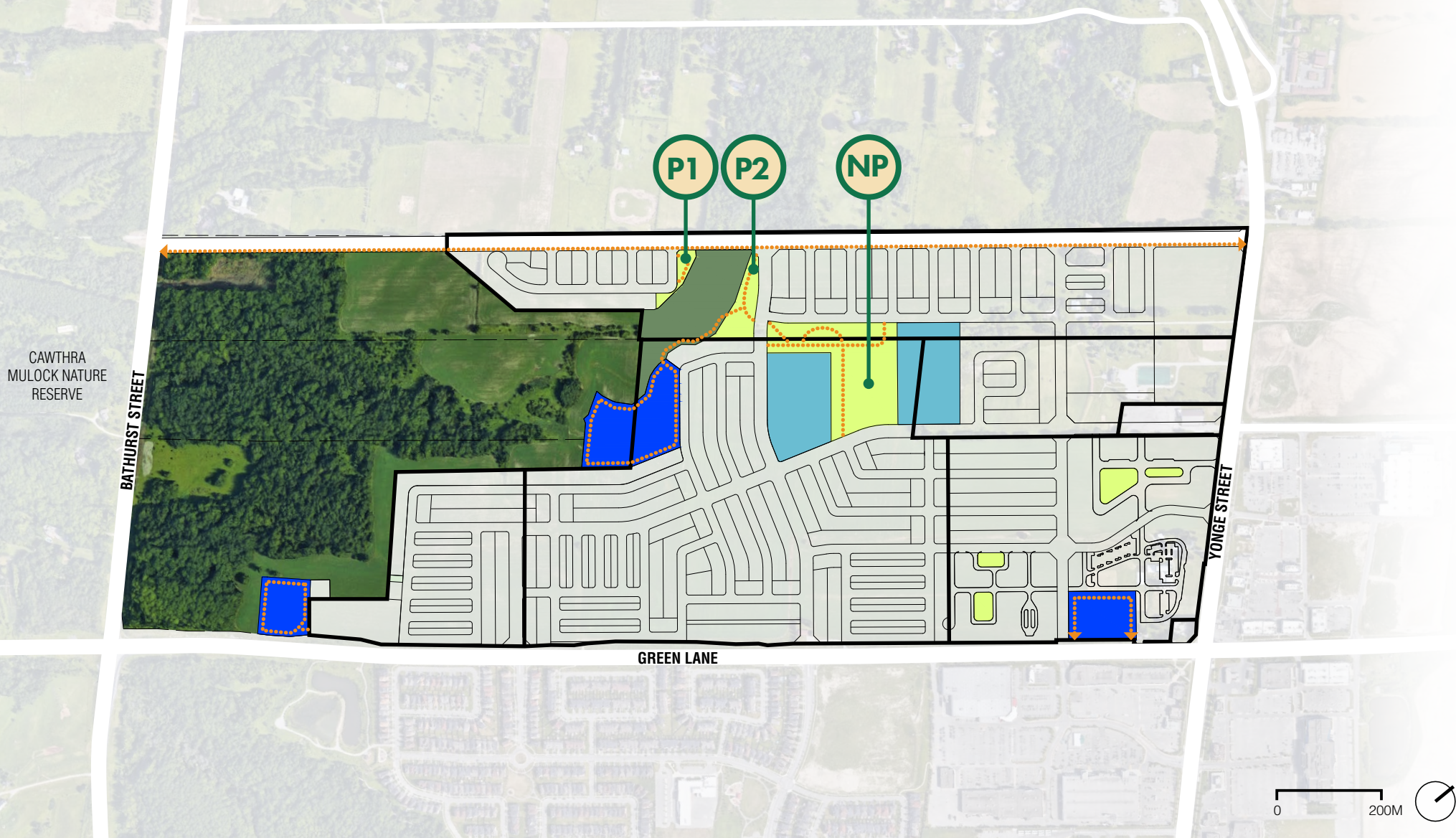
- All new institutional and public buildings shall be accessible to persons with disabilities, in accordance with the Ontario Building Code and AODA;
- Pedestrian networks will be designed to meet the Town's accessibility standards with minimal slopes, curb ramps, sufficient width, markings, tactile warning strips and guards to support accessibility and safety;
- Pedestrian networks shall be designed as barrier-free routes consisting of continuous and direct walkways, slip-resistant surfaces, trundle domes at roadway crossings, minimal interruptions from access driveways, and free of abrupt changes in grade;
- All street furniture (including benches, planters, waste receptacles, newspaper boxes, etc.) and landscape planting shall be located outside of the main pedestrian networks to ensure clear unobstructed walkways;
- The design of public spaces should consider accessibility and universal design to ensure the space is available for all residents and visitors to use. Parks and recreation facilities will be developed with innovative, high quality, barrier-free amenities and features that support accessibility and inclusion; and
- Appropriate AODA wayfinding signage should be provided.



Figure 5.1k: Accessibility features in the public realm ease navigation for those with varying mobility.



Figure 5.1l: Wayfinding signage and seating that is visible and accessible in accordance to AODA.



LEGEND

- Site Boundary
- Park/Amenity
- School
- Environmental Protection Area
- Stormwater Management Facility
- Servicing Block
- Potential Active Transportation Network

Figure 5.2a: NW Yonge Green Lane Proposed Greenspace

5.2 Greenspace

A connected system of parks and open spaces has been designed to provide a range of passive and active recreation opportunities within walking distance of all neighbourhoods that contributes to community character and identity within NW Yonge Green Lane.

The planning and design of parks and open spaces shall align with the objectives and guidelines in Town OP Policies *4.1 Environmental Protection Area* and *4.8 Parks and Open Space* and the Town's Parks Design Standards Manual (PDSM).

5.2.1 Linkages and Pathways

Safe and convenient pedestrian connections shall be a primary component of NW Yonge Green Lane. A sidewalk network shall tie all community amenities, parks and, potentially, the Greenbelt system, to provide safe and convenient connections throughout. In accordance with the Town's OP and Schedule E-1, regional cycling facilities are proposed on Yonge Street and the eastern portion of Green Lane. An existing cycling facility is located on the western portion of Green Lane West. Along the north, a cycling facility is proposed for the future major collector road. As per Schedule F, a proposed trail is also indicated through the NHS adjacent on the west side of the subject lands.

The trail and cycling network shall be consistent with the applicable guidelines including the Association of Pedestrian and Bicycle Professionals' Bicycle Parking Guidelines, 2nd Edition (2010), and the Accessibility for Ontarians with Disabilities Act (AODA). Bicycle and pedestrian path designations within NW Yonge Green Lane have been identified as follows:

- **Recreational Trails** - vary in width and surface treatment; may be incorporated into the community park, SWM facilities, and potentially through the Greenbelt lands (e.g., NHS trails are typically limestone screenings trail at 2.4m wide);
- **Walkway Blocks** - may be incorporated between dwellings to

facilitate an active transportation connection to a street, trail, open space, or neighbourhood;

- **Proposed Cycling Facilities on Regional Roads and Right of Ways** - As designated in the Town OP, facilities include, but are not limited to: multi-use trails, bike lanes, paved shoulders, signed routes, and other amenities to be determined through co-ordination between the Region of York and the Town;
- **Proposed Cycling Facilities on Non-Regional Facilities** - As identified in the Town OP, facilities include, but are not limited to: multi-use trails, bike lanes, paved shoulders, signed routes, and other amenities to be determined through co-ordination between the Region of York and the Town; and
- **Multi-use Trails** - Existing multi-use trail on the south side of Green Lane West.

5.2.2 NHS/Greenbelt Interface

Careful and seamless integration shall be considered for the transition between the natural edges and the built environment of NW Yonge Green Lane ensuring appropriate connections and compatible built form. The interface between the Greenbelt and adjacent proposed development will require careful consideration with respect to existing topography, vegetation communities, and hydrologic features and functions.

- Landscape buffers, where required, may contain a combination of native deciduous and coniferous trees, shrubs and grasses;
- A planting palette for transitional planting within buffers, stormwater management facilities, and other introduced features shall be utilized and consist of native species that is compatible with the existing or proposed plant material found within any natural features along the Greenbelt edge; and
- Fencing will be required between private property and the Greenbelt to help control access and prevent encroachment into the system.



Soccer Field



Half Court Basketball Court



Outdoor Fitness



Play Structure, Shade Structure and Seating

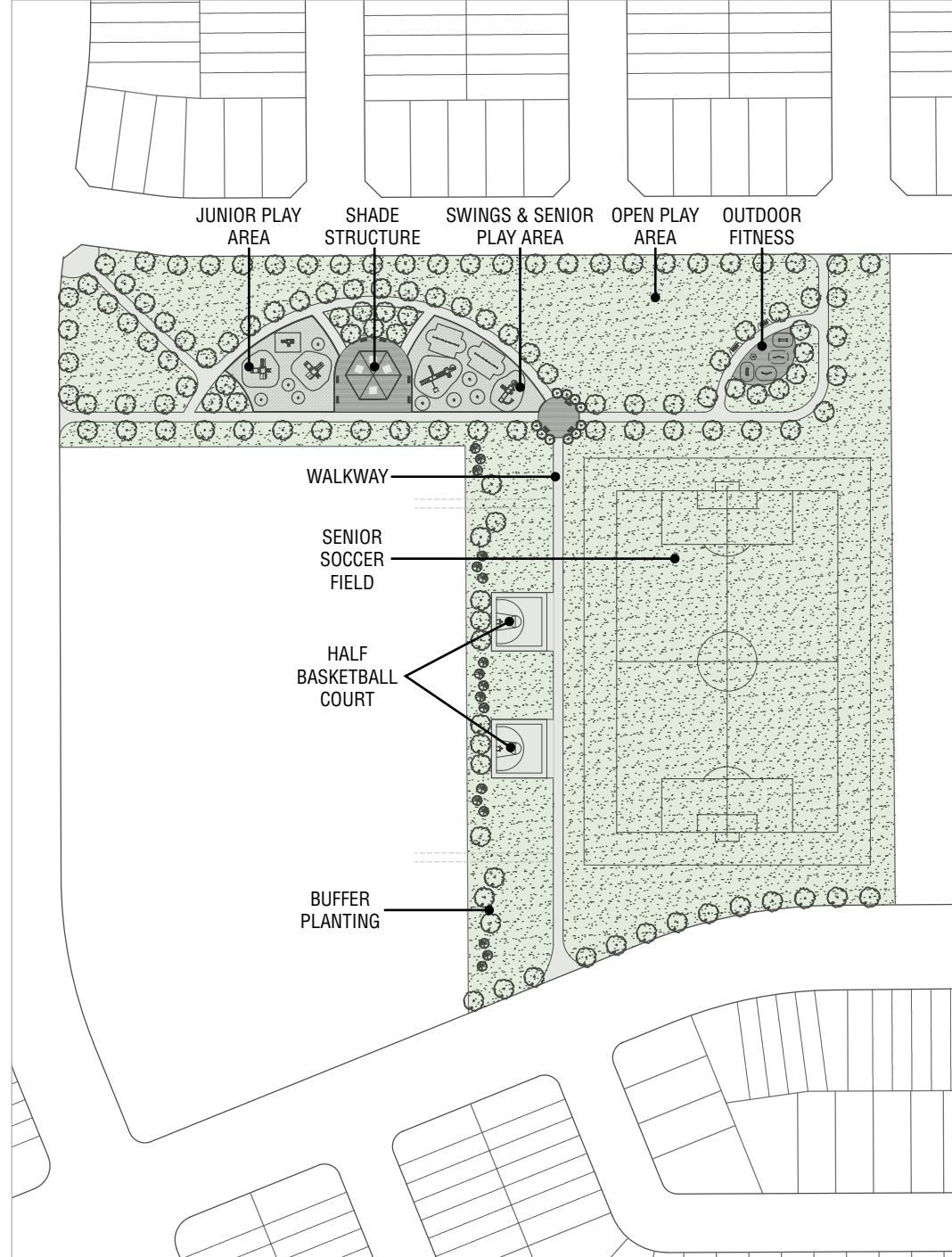


Figure 5.2b: Conceptual NW Yonge Green Lane Neighbourhood Park Facility Fit

5.2.3 Neighbourhood Park

The Official Plan section 4.8.29 *Neighbourhood Parks* states that Neighbourhood Parks shall be located within an 800 metre walking distance of all residential uses, generally without crossing any arterial roads or natural barriers. These parks primarily cater to the recreational needs and interests of the residents living within their general vicinity. Residents can easily walk or bike to these parks.

Neighbourhood Parks generally comprise municipal parkland, containing a mixture of passive areas, sports facilities, informal and formal play areas, and may include natural areas. They serve a population of approximately 5,000 people and have a minimum size of approximately 2.0 hectares.

Within NW Yonge Green Lane, a Neighbourhood Park is centrally located to provide convenient access for all residents in the community. It is co-located with two (2) adjacent elementary schools for the strategic sharing of amenities.

Potential features within the Neighbourhood Park may include:

- Formal entries, shade structures, seating, and decorative paving;
- Open grass areas with opportunities for unstructured play and flexible programming;
- Multi-use path(s) with direct connections to the street and pedestrian networks;
- Active sports facilities (e.g., basketball courts, etc.);
- Spray pad or hardcourt play;
- Playground facilities (e.g., swings, junior/senior play structures, spring/spinning toys, etc.);
- Outdoor fitness facilities; and
- Formal planting layout.



Figure 5.2c: A central community/neighbourhood park with a range of programming and active and passive facilities.

Design Guidelines

- The Neighbourhood Park shall be predominantly soft landscaped to allow for a variety of active and passive uses, including programmed and unstructured uses;
- The Neighbourhood Park shall be planned and designed as the central focus of the surrounding neighbourhood;
- As a focal point within the community, the park shall be sited with frontages on a minimum of two (2) public streets or lanes to promote views and access;
- Playgrounds and/or shade structures (including play structures, swings, etc.) shall be designed as a major focal element of the park; and
- Although the park is within walking distance of the surrounding catchment area, on-street parking within 50-100 metres of the park shall be provided.



Figure 5.2d: A parkette with active and passive play opportunities.



Figure 5.2e: A parkette with a shade structure and seating opportunities.

5.2.4 Parkettes

Parkettes are smaller open space features serving a primarily focal and social role for the surrounding neighbourhood, with potential for active play. The parkettes will contribute a strong visual identity for the surrounding neighbourhood and supplement the features and facilities found within the community/neighbourhood park. Reflecting the UDG direction for parkettes, the proposed parkettes in NW Yonge Green Lane are generally under two (2) hectares (4.9 acres). In addition to the broad design directives provided in the UDG, the following design guidelines should be considered:

Design Guidelines:

- Integration with surrounding elements should ensure a cohesive public realm system;
- Unprogrammed lawn areas should be flexible for passive and active use;
- Seating opportunities should be integrated within parkettes to promote and facilitate social interactions and community engagement;
- Shade structures with seating and decorative paving should be a primary focus for gathering;
- Planting beds should be optimized to accommodate tree soil volume, with reduced emphasis on soil cells; and
- Art installations may be considered to add unique placemaking elements to the development.

Refer to Figures 5.2 h and 5.2i on the following pages for conceptual parkette facility fit plans.

5.2.5 Privately Owned Public Spaces / Courtyards

In addition to the identified parks, squares, and natural features, an important component of the open space network is the collection of private open spaces/courtyards situated within development blocks within the Major Local Centre. While privately owned, these spaces will enhance the public realm by maintaining a strong relationship with adjacent streets and publicly accessible open spaces. They will increase opportunities for greenery and permeability, while achieving a balanced integration with the surrounding building massing and densities.

The design and programming of these spaces will be informed by the configuration of the framing buildings, their ground floor uses, the architectural design theme, access and circulation, programming opportunities, and relationship with adjacent public realm. There are a myriad of opportunities to create a diversity of spaces that will contribute positively to the open space character of NW Yonge Green Lane.

Design Guidelines:

- Shade structures with seating and decorative paving should be a primary focus for gathering;
- Seating and decorative paving should be the primary entry elements;
- The pattern of paths shall help frame the spaces and uses;
- Unprogrammed lawn areas should be flexible for passive and active use; and
- The formal and informal layout of trees should define paths and provide shade.

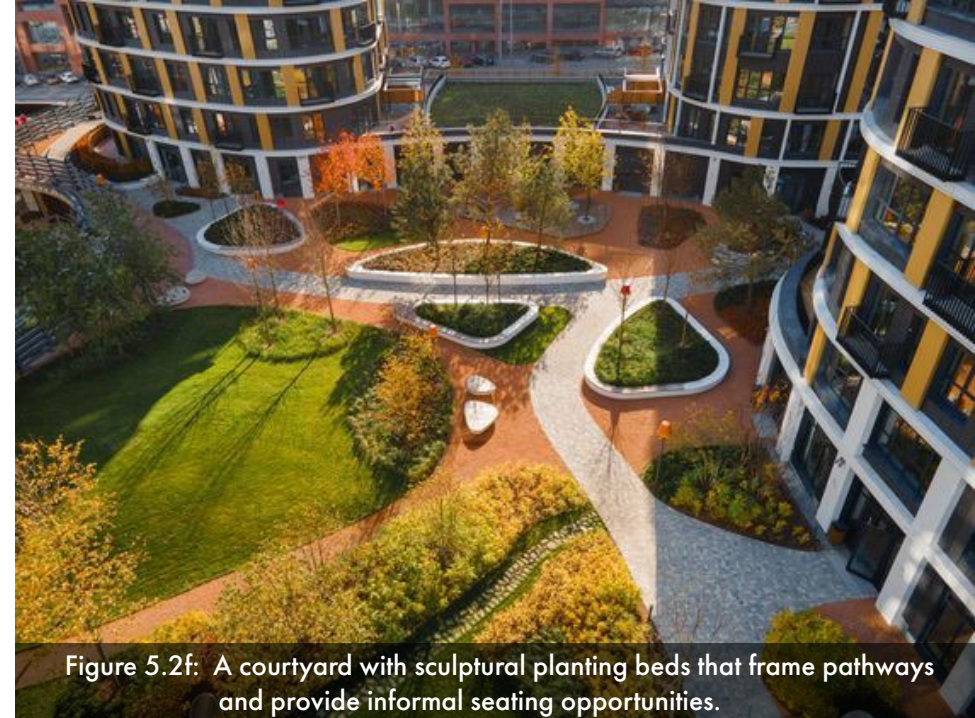


Figure 5.2f: A courtyard with sculptural planting beds that frame pathways and provide informal seating opportunities.



Figure 5.2g: Unprogrammed lawn area that allows for flexible and passive uses.

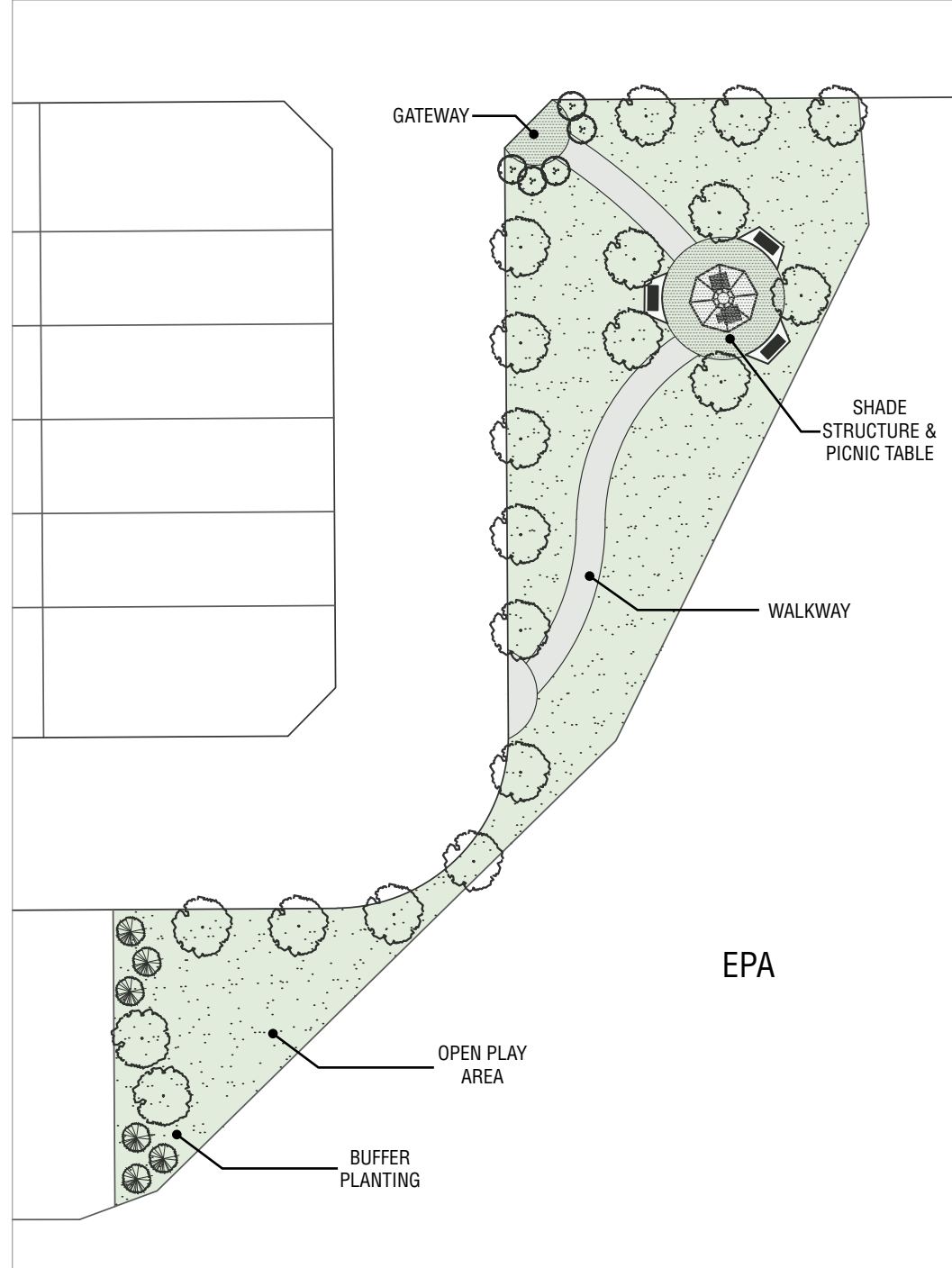


Figure 5.2h: Conceptual NW Yonge Green Lane Parkette 1 Park Facility Fit

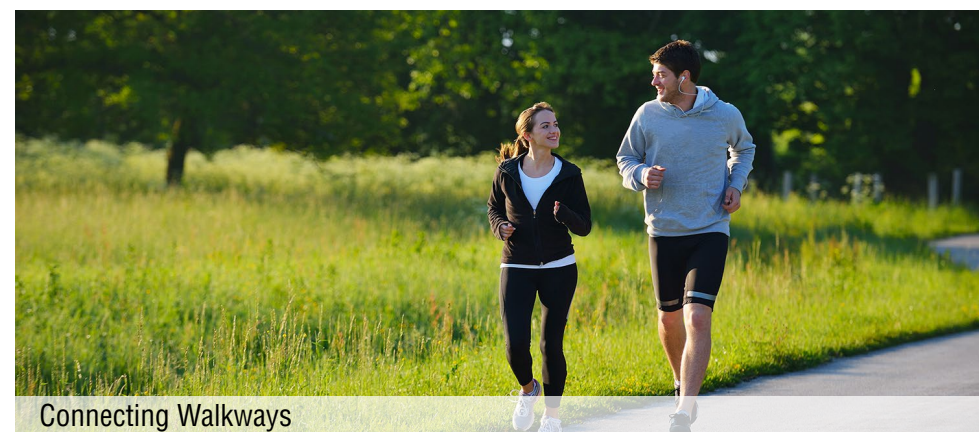
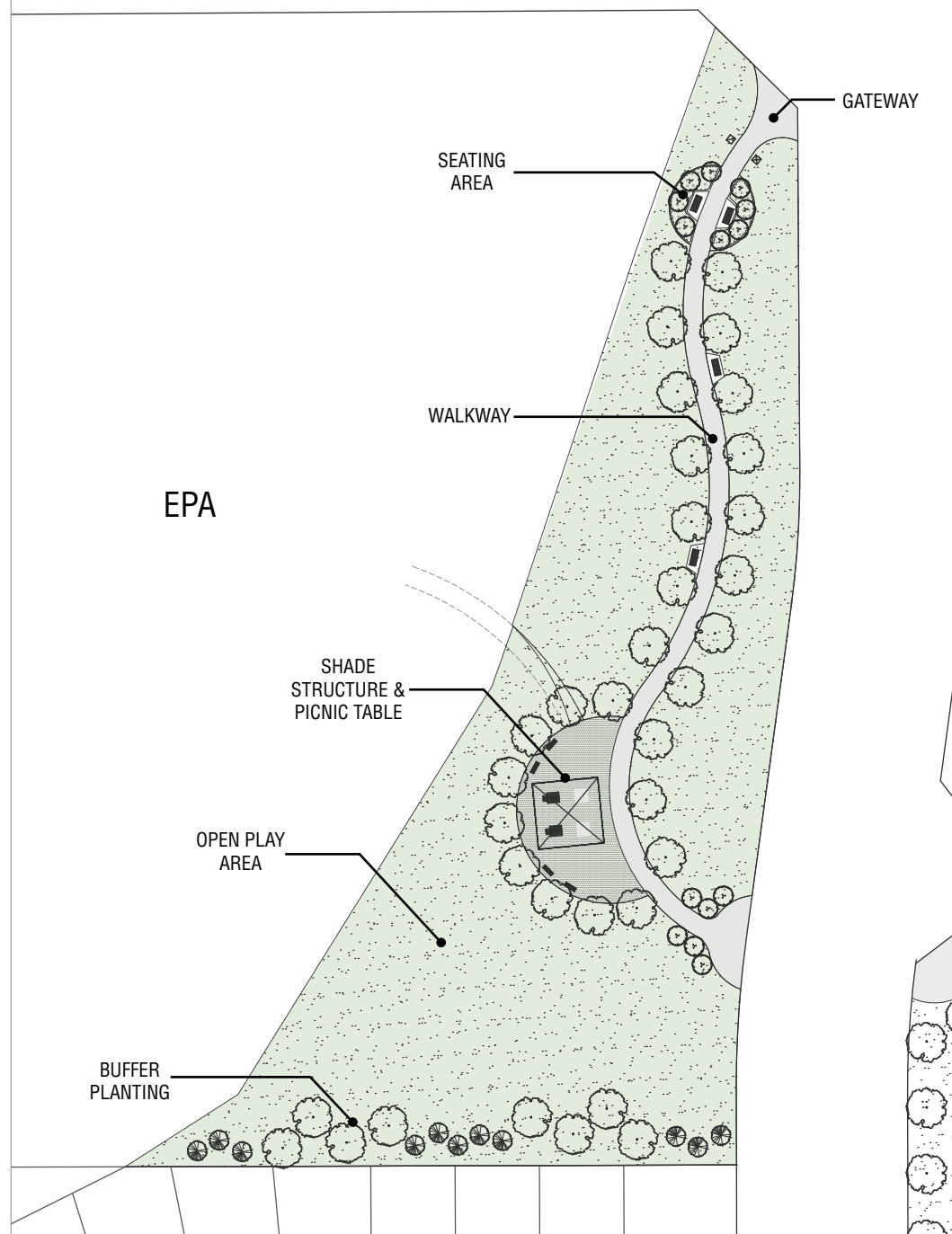


Figure 5.2i: Conceptual NW Yonge Green Lane Parkette 2 Park Facility Fit

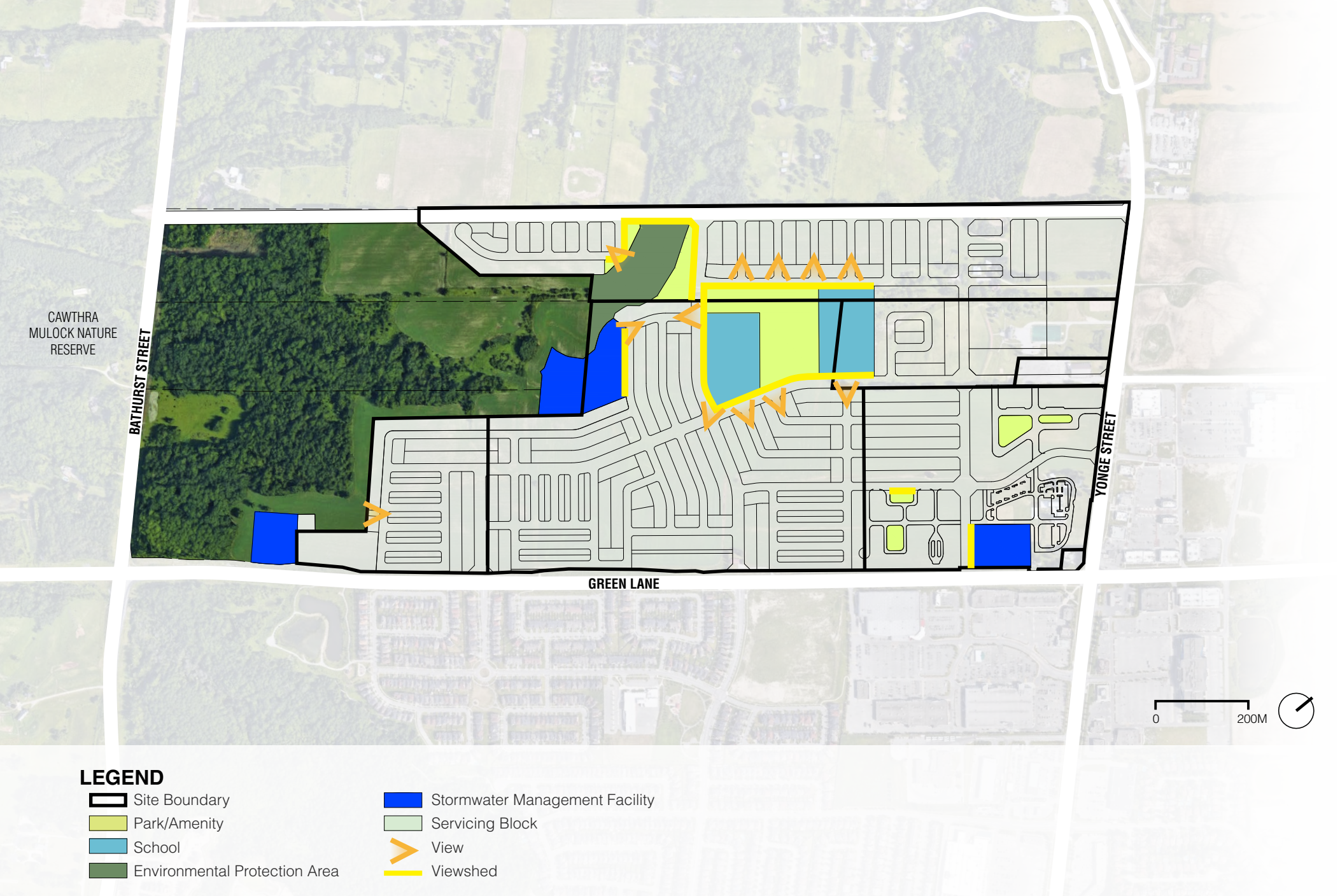


Figure 5.2j: NW Yonge Green Lane Proposed Views and Vistas

5.2.6 Views & Vistas

Public access to views and viewsheds is an integral component of an attractive, walkable and sustainable community. Within NW Yonge Green Lane, key views will be provided toward the NHS/Greenbelt, parks, and stormwater management facilities. These features will provide attractive views from various vantage points within the community. Preserving views has influenced the configuration of the proposed land uses, including the layout of the road network and the precinct plan.

Strategic viewshed opportunities have been integrated into the community through consideration of the following principles:

- Streets have been oriented to provide views towards open space features;
- Emphasis has been placed on providing access points to natural features by locating pedestrian amenities (lookouts and/or a trail system) along potential view corridors; and
- Architectural built form shall be located, oriented, and designed to maintain or emphasize views.

Through the application of these principles, viewsheds and corresponding views have been identified in order to guide the design of the surrounding urban fabric. As such, viewsheds are defined as publicly accessible viewing opportunities either along a road ROW, a trail network, or an open space block (parks, SWM facilities). The quality and character of the resulting view opportunity can be described as either long / expansive views, which typically afford an extensive vista or longitudinal view over a large distance, or short views, which are usually framed by a woodland edge or have built community features (roads, built form, etc.) in the background.



Figure 5.2k: Viewshed from a lookout toward a SWM facility.



Figure 5.2l: Viewshed from a park toward adjacent NHS.



Figure 5.2m: A pedestrian promenade next to an urbanized SWM facility.



Figure 5.2n: A naturalized SWM facility.

5.2.7 Stormwater Management Facilities

In addition to its primary water quality/quantity control and recharge functions, the three (3) proposed SWM facilities are designed to fulfill a secondary role as a community benefit. By complementing the parks and open space system through integration with the pedestrian/trail network, the SWM facilities will provide a neighbourhood amenity for primarily passive recreational use.

At the south east corner of the community, a proposed SWM facility is located within the Major Local Centre with frontage along Green Lane West. On the west, the second SWM facility is situated within the Greenbelt lands, with frontage on one side along a local road. The third SWM facility is located west of the site boundary, adjacent to the Greenbelt lands with one frontage along Green Lane West. These facilities have been situated in relation to existing drainage patterns of the development lands, and will provide additional viewshed opportunities of a naturalized open space. The following guidelines may apply:

- Naturalized planting throughout shall consist of whips, multi-stem shrubs, ornamental grasses, and riparian, aquatic, and upland species appropriate for the pond (dry) condition, with an emphasis on native species, in accordance with Conservation Authority standards;
- Pond inlets and outlets shall be concealed using planting, grading and/or natural stone;
- Should pedestrian access into the pond area be desirable and appropriate to the surrounding residential or commercial land uses, the maintenance/access roads may facilitate these connections;
- Fencing requirements for the ponds will be determined, in part, by the interface condition with the surrounding residential lands to the north and west and potential future commercial lands to the south;
- Areas for seating may be integrated with pedestrian connections where grading and visibility allow;

- Should utility structures be placed within the pond facility, they should be screened from public view with planting and fencing or other built features, as necessary;
- Dense planting shall be used to discourage access to sensitive landscape areas or those inappropriate for public use;
- Information signage shall be provided within areas of high visibility to inform the public of the importance and treatment of the stormwater management facility as a functioning natural open space feature;
- Public access to areas of the SWM facilities that are appropriate for pedestrian connections and viewing opportunities shall be provided;
- The zone between the street and stormwater management facility shall be designed as a transition from an urban streetscape to a naturalized area;
- Lookout features shall serve as a community amenity, and will typically include decorative paving, seating elements (benches and/or seat walls) and upgraded planting, to be coordinated with neighbourhood themes. The amenity may also integrate a shade structure;
- Fencing of the SWM facility adjacent to publicly accessible areas is discouraged. However, where it is desirable to discourage public access to the pond, barrier plantings and living fences consisting of plant material may be utilized in place of fencing; and
- Further discussion will be required at the detailed design stage to contemplate the proposed urban interface condition.

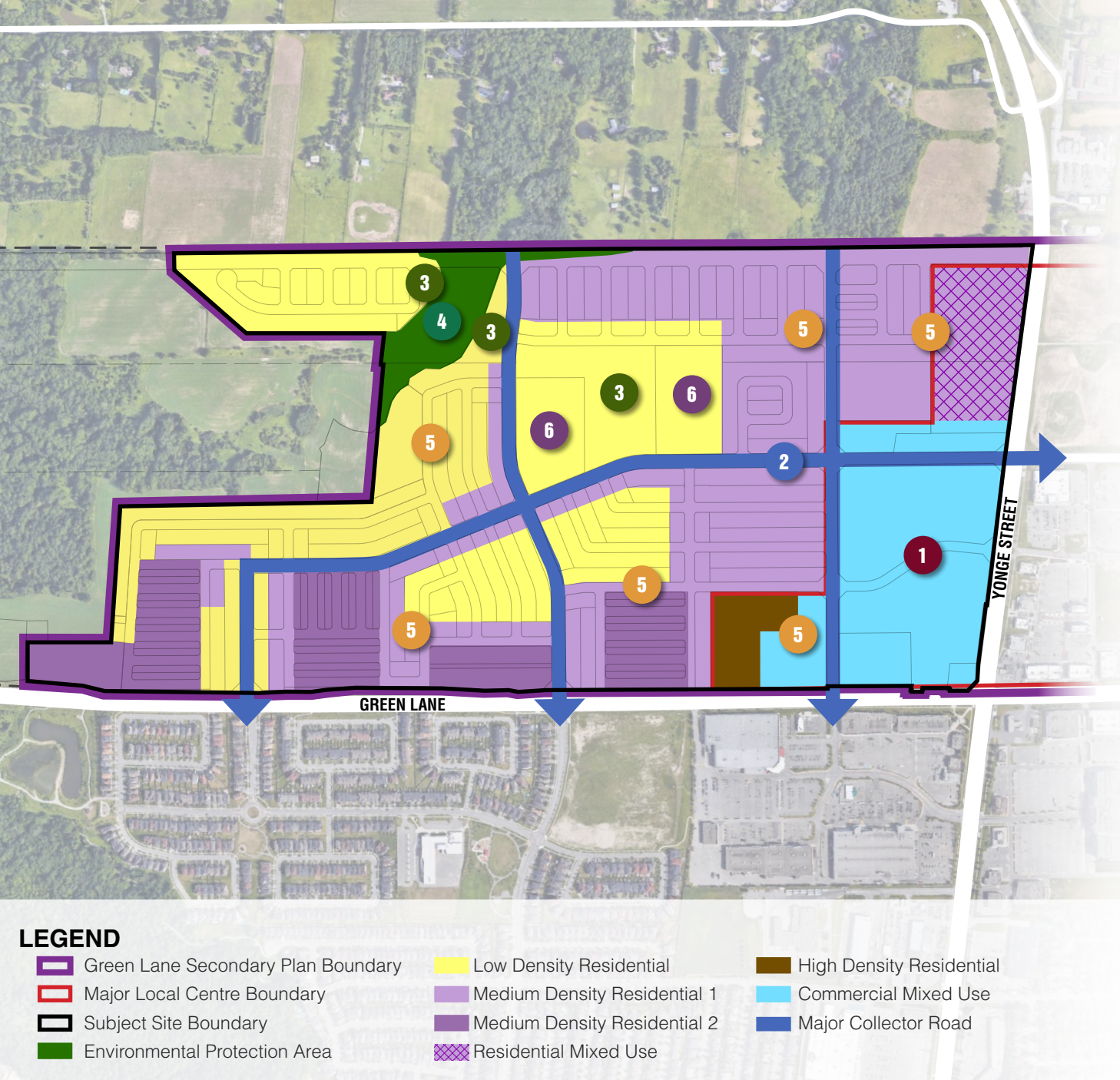
Refer to Figure 5.2a for a plan showing the planned trail and cycling connections in relation to the SWM facilities within the NW Yonge Green Lane. For further design criteria, reference Section 7.4 Stormwater Management Facilities of the Town OP.



Figure 5.2o: A SWM facility with naturalized planting and native species.



Figure 5.2p: Opportunities for public access through lookout features.



- 1 Local Major Centre** - A strategically located mixed-use area with an urban village character and a variety of locally serving retail and services.
- 2 Connected Network** - A comprehensive linked network of street, trails, cycling facilities that knit together the existing and proposed neighbourhoods with open space amenities and services.
- 3 Neighbourhood Park/Parkette** - A community destination that provides neighbourhood programming, amenities, education and recreational activities.
- 4 Open Space System** - A robust open space system that reinforces preservation of natural features and the integration of extensive active and passive recreation parks.
- 5 Diversity of Housing** - A mix of housing options that will appeal to a wide spectrum of residents.
- 6 Schools** - Elementary schools are provided in line with Town requirements to support the growing community.

Figure 6.1a: NW Yonge Green Lane Proposed Community Structure

urban neighbourhood structure, form & design

6

6.1 Community Structure

The structuring elements for the future community will serve as the main building components for delineating the various land uses, establishing the street hierarchy network and providing the framework of land uses in NW Yonge Green Lane. Consistent with the design parameters set forth in Town policies, the existing and proposed network of Regional and Local Corridors functions as a key structuring element for establishing the configuration of land uses, built form locations and streetscape features. The proposed development contains:

- **Local Major Centre** - A commercial and residential mixed-use, and medium/high density residential area;
- **Connected Network** - A hierarchy of streets in a modified grid pattern.
- **Community/Neighbourhood Park** - A central focus of the community, co-located with the proposed schools;
- **Open Space System** - Stormwater management facilities, Greenbelt Plan Area; parkettes;
- **Diversity of Housing** - Low, medium and high density residential dwellings; and
- **Schools** - Two (2) proposed elementary schools;

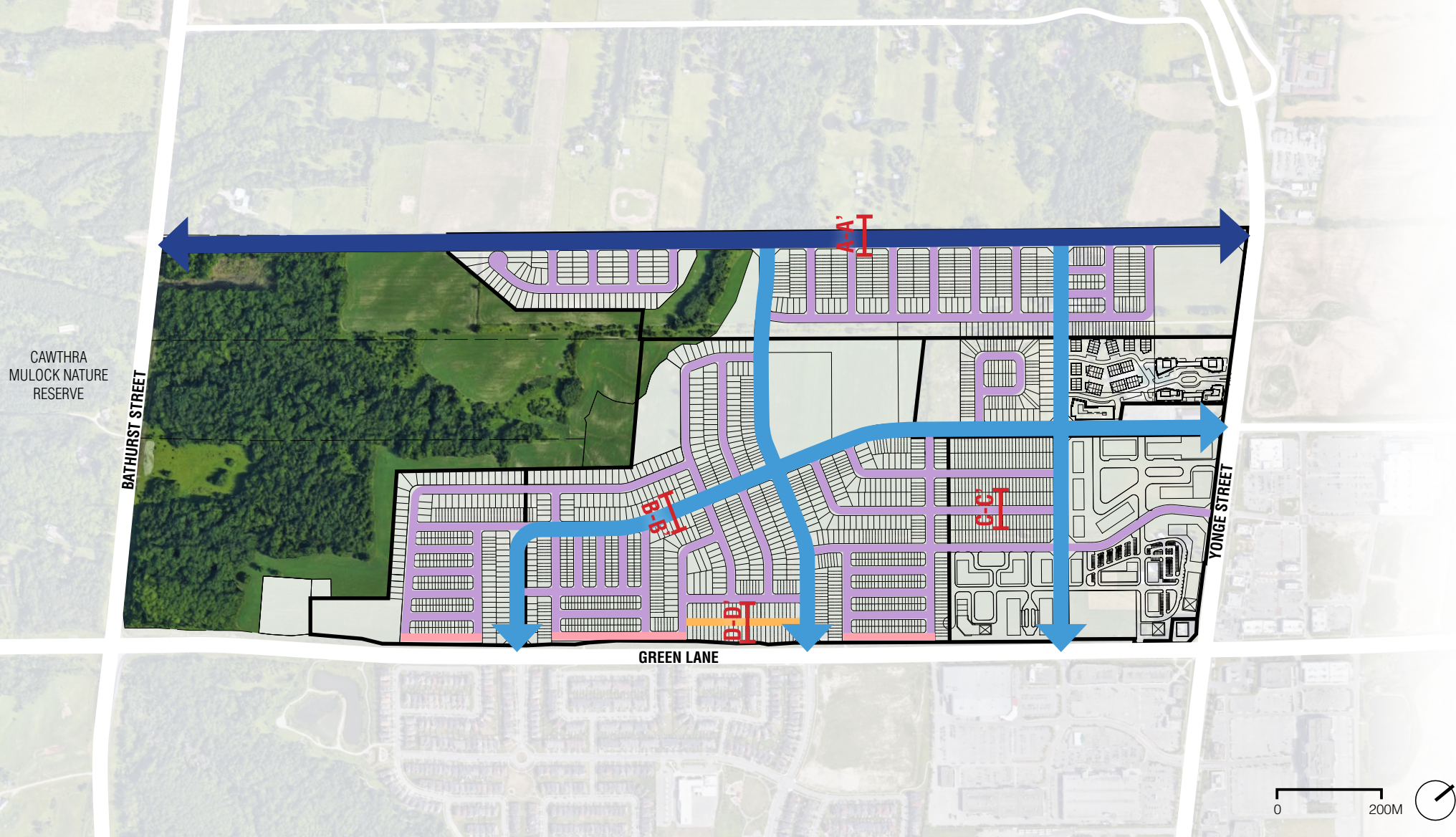
6.1.1 Greenspace System

Complementary to the Town's overall planning and coordination of amenities, a series of active transportation connections and open spaces are proposed within NW Yonge Green Lane. The system of parks and open spaces is intended to provide a range of passive and active recreation opportunities within walking distance for all residents. Important views and viewsheds, combined with linkages to the trail network will enhance permeability through the community and connectivity between its open spaces and the Town-wide parks system.

To encourage walking and cycling trips to nearby open space, transit, and commercial amenities, the site shall be designed to create safe, direct, and efficient pedestrian links to the mixed-use blocks, and the surrounding existing and future active transportation network.

6.1.2 Neighbourhoods

The structuring elements help to establish the framework for neighbourhoods within the subject lands. In particular, the minor collector road networks provide the basic structure for identifying individual neighbourhood areas. With the proposed Neighbourhood Park centrally located in the subject lands, the majority of residents are within an approximate ten-minute (800m) walking radius of this important community amenity. The Town applies this walking distance metric for the Neighbourhood Park designation to ensure that residents within the broader neighbourhood have convenient access to an outdoor recreational facility and green space.



LEGEND

- | | |
|----------------------------|---------------------|
| Site Boundary | 18.0m Local Road |
| 30.0m Major Collector Road | 16.0m Window Street |
| 22.0m Minor Collector Road | 8.5m Laneway |

Figure 6.2a: NW Yonge Green Lane Proposed Street Hierarchy

6.2 Street Hierarchy

A well-defined, linked, and easily recognizable hierarchy of streets forms the structure of the community. Designed as a modified grid pattern, the street layout for NW Yonge Green Lane is intended to facilitate movement and circulation, support accessibility and transit ridership, and promote a safe pedestrian and cycling oriented lifestyle. The streets are also designed to minimize block lengths for easier navigation and create terminating views, vistas and other focal points to achieve an attractive public realm.

NW Yonge Green Lane's proposed street hierarchy is consistent with the framework provided in the Town OP, with the minor collector streets serving as the main north-south and east-west vehicular and pedestrian connections. Local roads (18.0m R.O.W.) are designed to reinforce a pedestrian focus and ensure safe connections within the community, connecting residents to amenities, such as parks and open spaces.

An Environmental Assessment (EA) completed by the Town identifies the major collector road as a potential 30.0m ROW, so cross sections will need to be developed during the draft plan detailed design stage. Figure 6.2b shows the Town standard 26.0m ROW for illustrative purposes only.

The following street types are located within the development:

- Major Collector Road (30.0m R.O.W.)
- Minor Collector Road (22m R.O.W.)
- Local Road (18.0m R.O.W.)
- Window Street (16.0m R.O.W.)
- Laneway (8.5m R.O.W.)

The following describes some of the proposed streetscape elements associated with each proposed roadway type:

6.2.1 Existing Arterial Roads

External arterial roads are designed to carry larger volumes of traffic at moderate to higher speeds over longer distances. Bounding the eastern and southern edges of NW Yonge Green Lane, Yonge Street and Green Lane West are both designated as regional arterial roads. Their future character varies according to planned land uses, which range from future higher density mixed-uses concentrated along Yonge Street and the eastern portion of Green West Lane, and transitioning to medium/low density residential further west along Green West Lane. The character for development along these roads is described as follows:

- The current streetscape of Yonge Street along the subject lands is characterized by rural conditions on the west side and a low-rise commercial plaza on the east. The existing R.O.W. includes two travel lanes in each direction, turning lanes, and a centre median.
- At the south edge of the subject lands, the existing character along Green Lane West is characterized by rural conditions along the north, and low-rise commercial plaza uses on the south. The existing R.O.W. includes two travel lanes in each direction and a centre median. The south side of Green Lane West includes a sidewalk and a multi-use trail.
- As per OP Schedule E-1 Cycling Network, 'Proposed Cycling Facilities on Regional Roads and Right of Ways' are designated on both Yonge Street and Green Lane West, which may comprise multi-use trails, on-street bike lanes, or signed bike lanes.
- Anticipated future conditions for these external arterial roads shall be determined and coordinated with Regional and Town staff as the subject lands become urbanized as part of this development.

6.2.2 Major Collector Roads

Major collector roads are designed to carry larger volumes of traffic and bus transit service at moderate to high speeds over long distances. Their streetscape character varies according to land uses. Should the current roadway width allow for future expansion of the roadway, the streetscape treatment planned for NW Yonge Green Lane will reflect this interface. The existing and proposed character for this arterial road is based on the NW Yonge and Green Lane Secondary Plan Area Urban Design Guidelines and is described as follows:

The existing character along the future Major Collector Road is predominantly existing rural residential and agricultural lands.

- Within the subject lands, the proposed character along the north side of the proposed Major Collector Road will be defined by the residential land uses and the landscape interface between the built form and the right-of-way.

Typical roadway cross sections for the 30.0m collector road right-of-way include:

- Sidewalks on both sides of the street;
- One (1) lane in each direction;
- Potential multi-use trails, on-street bike lanes, or signed bike lanes; and
- Single row of trees in grass boulevards between sidewalk and curb.

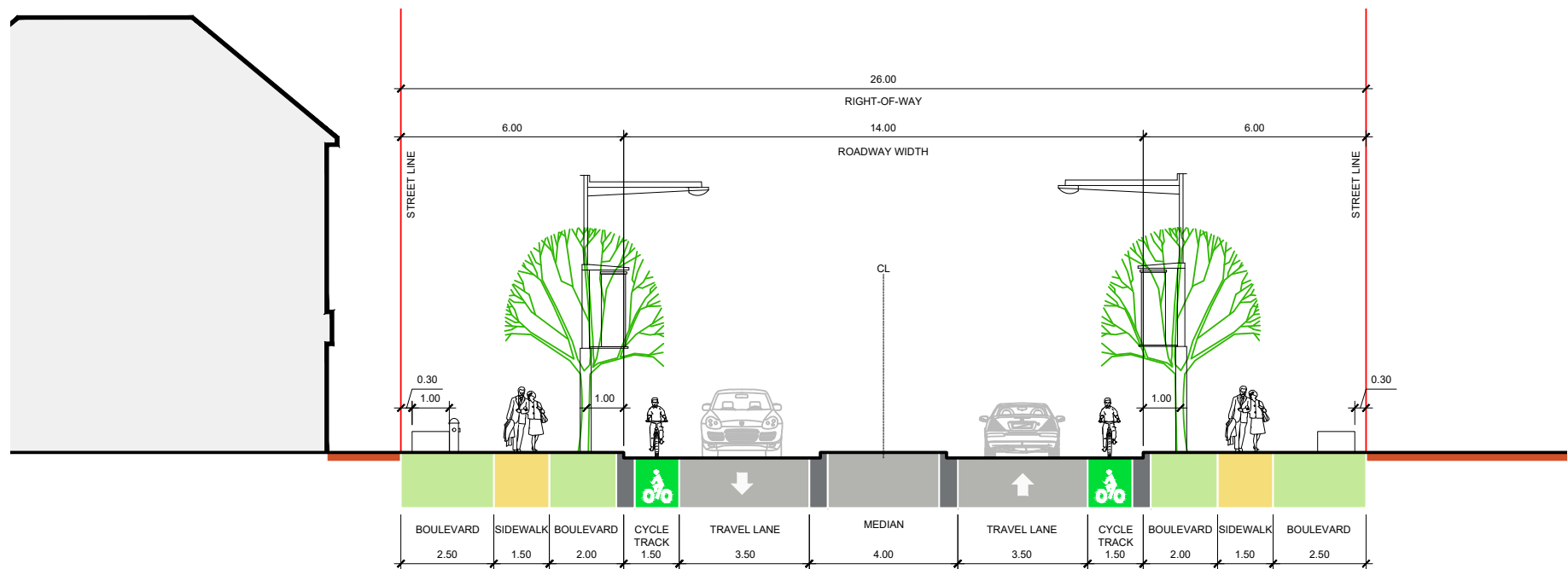


Figure 6.2b: A-A' Proposed Major Collector Road Cross Section (for illustrative purposes only)

6.2.3 Minor Collector Roads

Minor collector roads provide important connections between residential neighbourhoods and community functions, such as parks, schools, and other facilities. They largely define the community structure, serve as the primary inter-neighbourhood circulation routes.

An urbanized condition at the community gateways within the Major Local Centre may include special paving treatments, raised planters, and tree grates, street light poles and luminaires, and coordinated street furniture that reflect a unique community character, subject to approved Town standards.

The minor collector roads R.O.W. within NW Yonge Green Lane will be 22.0m in width. Streetscape character varies according to land uses, which range from mixed-use, townhouses, single-detached residential, parks, schools, and stormwater management facilities.

The cross section of this road includes 2 travel lanes, 1 parking lanes, boulevards, street trees, and sidewalks on both sides of the road. The streetscape may also incorporate enhanced pedestrian crossings at key intersections to reinforce a safe, walkable environment.

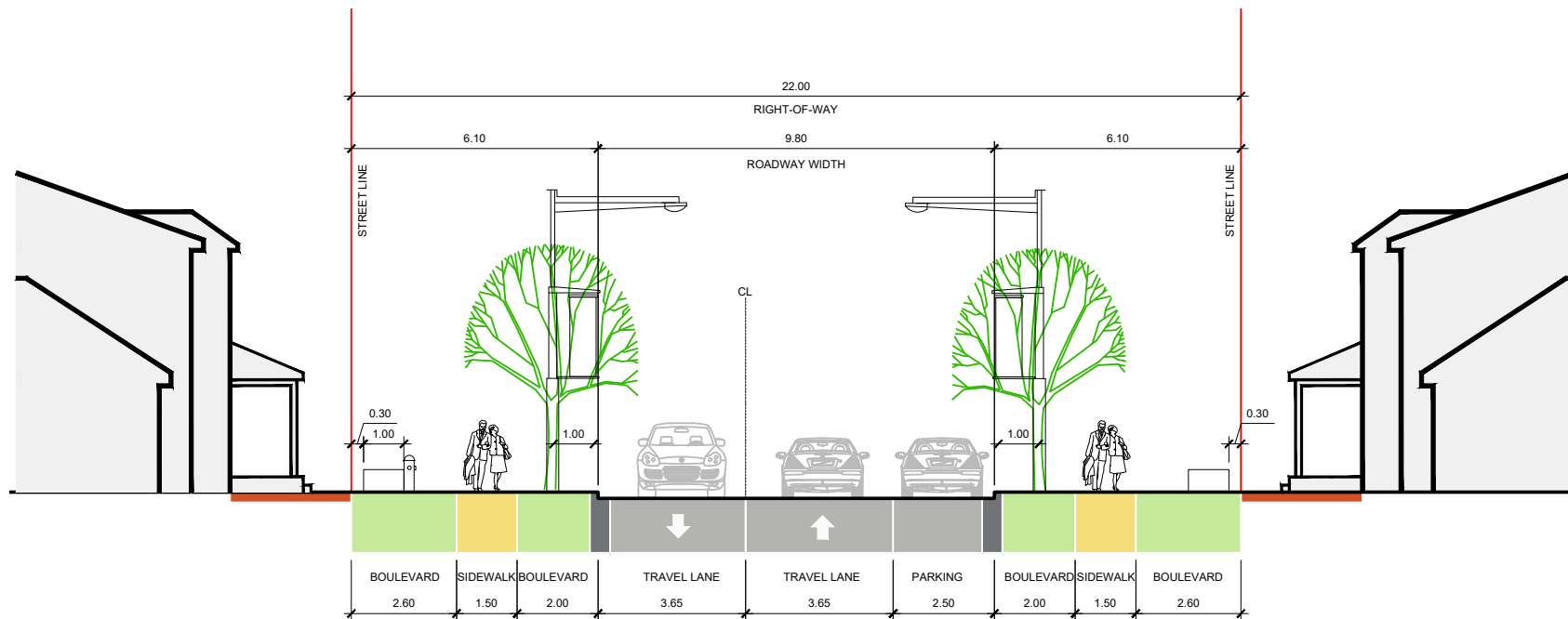


Figure 6.2c: B-B' Proposed Minor Collector Road Cross Section

6.2.4 Local Roads

Local roads will primarily serve the low rise residential areas and are intended to provide a comfortable pedestrian experience with relatively low levels of local vehicular traffic. Their character varies according to adjacent built form, which may include low and mid-rise residential built form, parks, stormwater management facilities, and frontage on the Greenbelt. The local road network shall facilitate logical, direct, permeable, and safe neighbourhood connections through a modified-grid configuration. The use of cul-de-sacs shall be minimized throughout the community. Local roads are intended to have an 18.0m R.O.W. with two travel lanes (one lane in each direction). 18.0m R.O.W.s are demonstrated in the cross sections below, however final R.O.W. widths will be examined further in future studies.

- A 4.5m wide boulevard will be provided with 1.5m sidewalks on both sides and a single row of street trees.
- Single-loaded roads may be proposed in areas adjacent to the Greenbelt, SWM facility, or hydro corridor.
- Single-loaded roads generally have a narrower R.O.W. with one lane in each direction and a sidewalk or multi-use paths on one or both sides.
- The boulevard treatment on single-loaded roads consists of street trees on the dwelling side boulevard and trees with buffer planting within a grass boulevard adjacent to the arterial road boulevard to provide views and access.

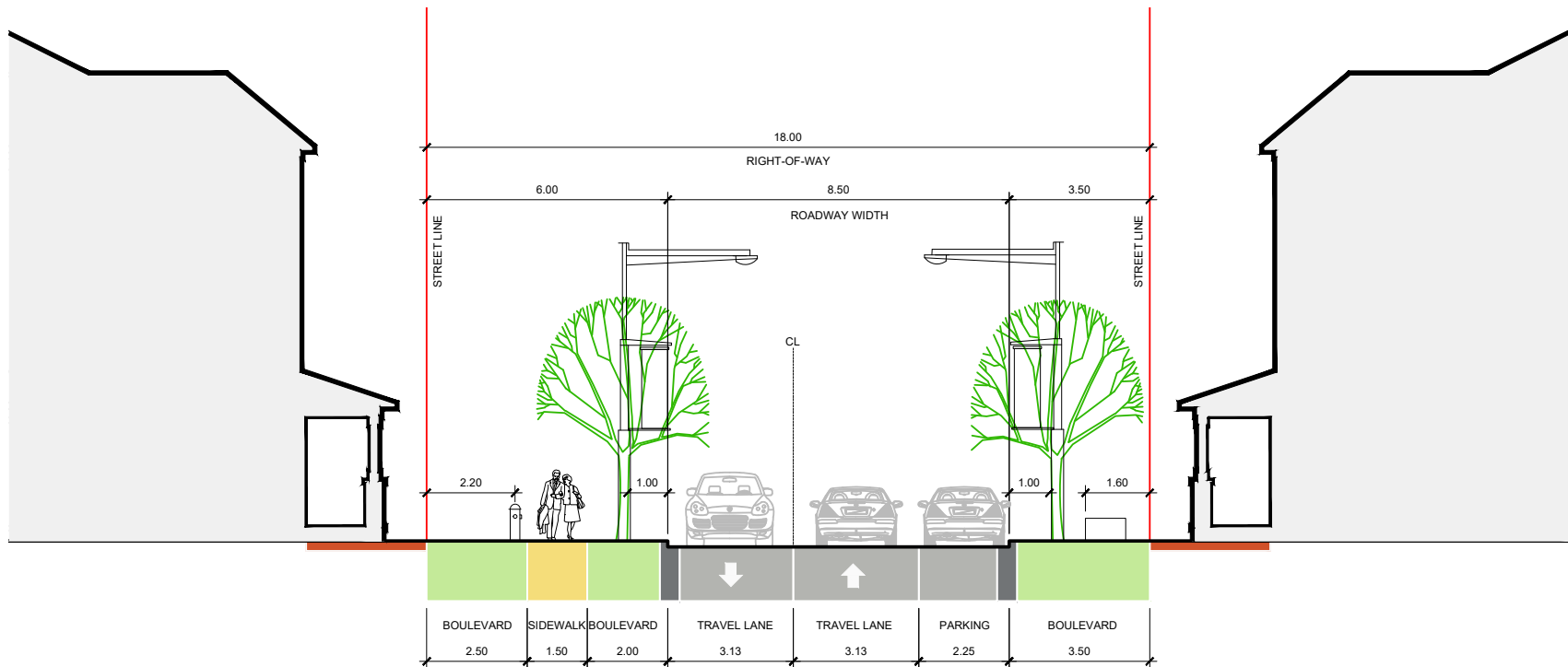


Figure 6.2d: C-C' Proposed Local Road Cross Section

6.2.5 Window Street

Window streets are proposed in particular situations to avoid residential reverse lotting and frontages of single family detached dwellings directly along the arterial road. The associated built form type along window streets in NW Yonge Green Lane are varying townhouses that front onto Green Lane West. With a landscape buffer adjacent to Green Lane West, the window street configuration provides noise attenuation for the dwellings and avoids a front door orientation towards the adjacent arterial road. They are intended to provide a safe and comfortable pedestrian experience with allowances for driveway access from the window street.

Typical 16.0m window street R.O.W cross sections will include:

- The boulevard treatment consisting of street trees on the dwelling side boulevard and trees;
- One (1) 1.5m sidewalk provided on the residential side; and
- Adjacent landscaping strip with buffer planting and low decorative fencing within a grass boulevard adjacent to the arterial road boulevard.

6.2.6 Laneways

Laneways may be proposed in the residential areas for townhouse and dwellings situated along arterial roads, on which driveways for individual units and lots are not permitted, as well as within contemplated residential/mixed-use condominium blocks.

- The laneway cross section will have an 8.5m R.O.W., featuring two travel lanes (one lane in each direction).
- Laneways will include a mountable curb and a concrete apron on both sides, and access to rear or flankage garage parking.

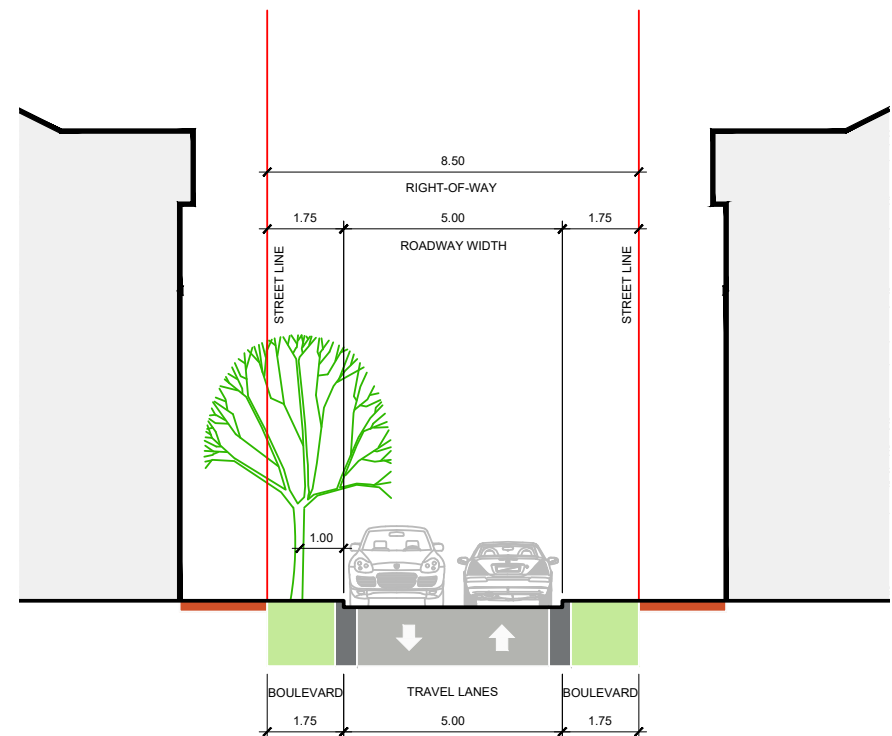


Figure 6.2e: D-D' Proposed Private Lane Cross Section



6.3 Built Form

A high quality built form character shall be achieved for NW Yonge Green Lane, delivering architecture that is rich and varied in its form and treatments, creating a distinctive community with visually appealing streetscapes. Aligning with the Town policies for the Major Local Centre, mixed-use and/or multi-storey buildings are planned near the Yonge Street and Green Lane West intersection to reinforce and promote a comfortable public realm and pedestrian-friendly streetscape. Throughout the development, built form shall have a strong orientation to the street with minimal setbacks to provide the appropriately scaled street edge on all public and condo roads.

Single detached dwellings comprise a significant portion of the built form along local roads and adjacent to the NHS interface. The townhouses fronting Green Lane West will be designed with regard to the prominent location on this regional arterial road within the Town, providing a streetscape condition that is predominantly urban in character.

6.3.1 Massing and Density

The community's built form shall include a range of densities, with high and medium density blocks establishing a compact, transit-supportive urban character. Massing and density that is appropriate to the context of the street is key to achieving a pedestrian-friendly, comfortable scale environment.

With the existing arterial roads serving as a framework for the development concept, higher density built form is generally associated with the edges of the community, primary gateways, and major roads. The proposed development strategically provides these high-rise and mid-rise built forms along community thoroughfares to create a streetwall that is appropriately scaled for these major streets and intersections.

6.3.2 Built Form Character

The development will encompass a variety of dwelling types and densities to accommodate a range of users and needs. Built form character, height, setbacks, and massing may vary according to use, with higher densities in the Local Major Centre, as they provide the critical population base and built form type to ensure support for amenities such as commercial and retail uses, community services, and transit ridership.

NW Yonge Green Lane will offer high quality built form that reflects the established character of the area, including utilization of architectural styles and treatments that promote vibrant pedestrian environments and help foster a distinct identity for the community as attractive, cohesive, and the next logical progression of growth in the Town of East Gwillimbury. The architectural styles and themes for each neighbourhood will be developed in a coordinated manner, in consultation with the stakeholders and the Town.

6.3.3 Built Form Typologies

Proposed building typologies within NW Yonge Green Lane may include:

Low/Medium Density Residential

- Single family residential dwellings
- Street townhouses
- Rear lane townhouses
- Back to back townhouses

Medium Density / Mid-Rise

- Residential mixed-use buildings
- Commercial mixed-use buildings

High Density

- High density residential buildings



Figure 6.3a: Mix of mid to high rise residential buildings



Figure 6.3b: Rear Lane Townhouses



Figure 6.3c: Single detached dwellings with 2-storey massing, prominent entries, and paired driveways.



Figure 6.3d: Street-facing garages incorporated into the main massing of the building.

6.3.4 Low-Rise Residential Buildings

6.3.4.1 Single Detached Dwellings

Single detached dwellings will be a feature of the community, occupying a wide range of lot sizes and frontages to ensure that there is diversity in the housing options available. The architecture, color palette, and landscaping of each home should complement the neighbouring homes and create a cohesive look. In addition to contributing to the overall aesthetic, single detached dwellings should also be designed to meet the needs of the community. This includes taking into consideration factors such as the age and lifestyle of the residents.

Design Guidelines:

- Building elevations visible from public areas should incorporate appropriate massing, proportions, wall openings and plane variation in order to avoid large, uninteresting façades;
- For corner units, both street facing elevations should be given a similar level of architectural treatment. Main entries for these dwellings are encouraged to be oriented to the flanking lot line;
- Corner lot dwellings should be a minimum of 2-storeys;
- Dwelling designs with covered front porches or porticos where appropriate to the architectural style are encouraged; and
- Attached street-facing garages should be incorporated into the main massing of the building to ensure they do not become a dominant element within the streetscape.

6.3.4.2 Street Townhouses

Townhome dwellings are an efficient use of land and an energy conservative housing form that will add built form diversity to the development of NW Yonge Green Lane. They are proposed to be located in areas of the development where a denser housing form is desired, and may occur on public streets or on private streets in the medium density blocks.

Street townhomes may be 2 to 3-storeys, and will typically have a single car, front-facing garage accessed from the street, accommodating 2 cars per unit (1 in garage and 1 on driveway).

Design Guidelines:

- Mixing of townhouse block sizes within the street can help provide visual diversity in the streetscape;
- Townhome block composition should display massing and design continuity, while achieving adequate elevation variety, where appropriate to a given architectural style;
- Facade articulation is encouraged to avoid large unbroken expanses of roof or wall planes;
- The main front entry should be oriented to the front lot line for interior units and to the flanking (exterior side) lot line for corner units; and
- Each townhome block should have appropriate façade detailing, materials and colours consistent with its architectural style.



Figure 6.3e: 2-storey street townhouses in a modern style with prominent front entryways.

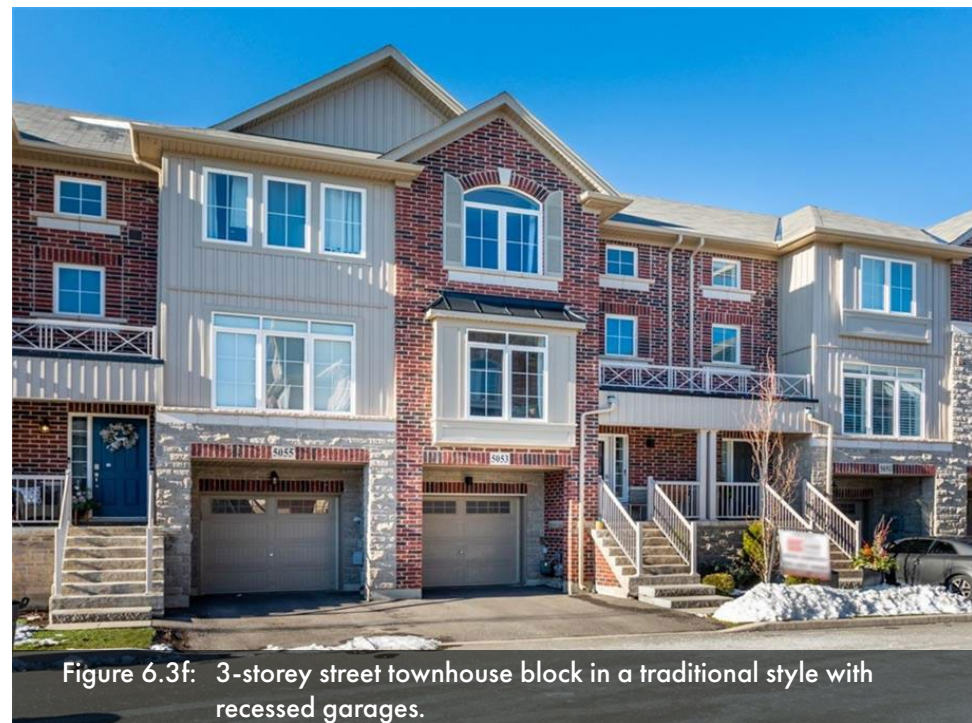


Figure 6.3f: 3-storey street townhouse block in a traditional style with recessed garages.



Figure 6.3g: Traditional style back to back townhouses with well-detailed balconies facing the street.

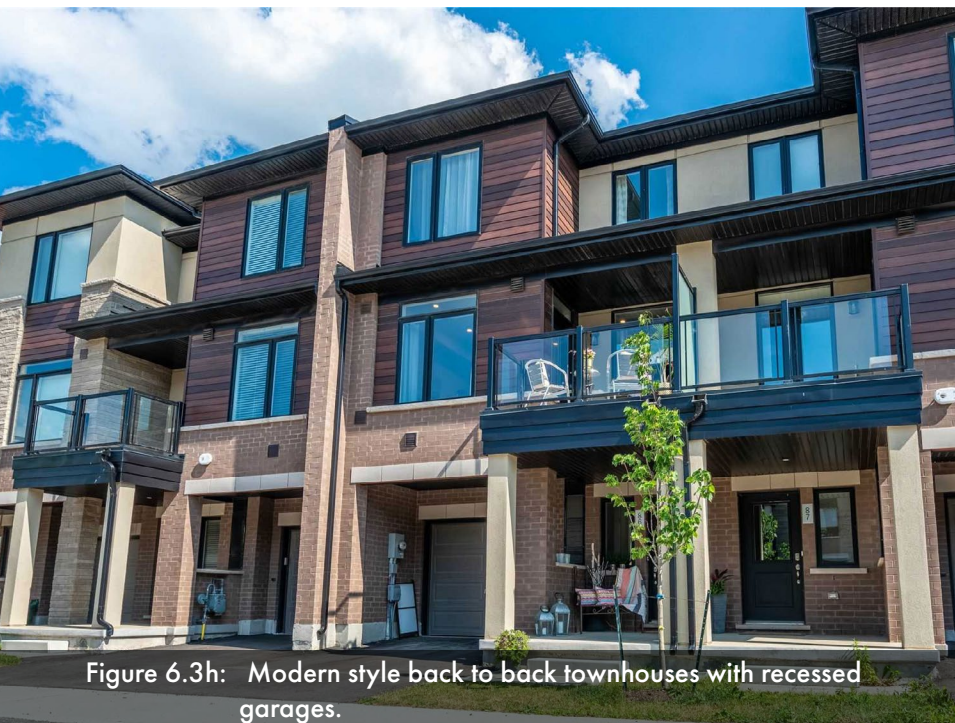


Figure 6.3h: Modern style back to back townhouses with recessed garages.

6.3.4.3 Back to Back Townhouses

Back to back townhomes may occur on public streets or on private streets in NW Yonge Green Lane. This type of townhouse is typically a 3-storey housing form with front facing garages accessed from a public or private road. As the name suggests, there is a common demising wall along the rear of the unit in addition to the traditional interior side walls. Outdoor amenity space is provided in the form of a balcony.

Design Guidelines:

- Mixing of townhouse block sizes along the street can help provide visual diversity of the streetscape;
- Privacy screens should be provided between outdoor amenity spaces of adjacent units;
- Since balconies will be facing the street, they must be well-detailed to suit the architectural style of the building using upgraded, durable and low-maintenance materials;
- Façades should be developed to incorporate architectural elements found on lower density housing forms such as peaked roofs, gables, porches and roof overhangs;
- Flat roofs and/or rooftop terraces are permitted;
- Garages may not project beyond the front wall or porch face of the dwelling;
- Utility meters and air conditioning units are encouraged to be located away from immediate public view, where possible; and
- Entrances to each unit should be ground-related requiring no more than a few stairs to access, subject to site grading conditions.

6.3.4.4 Rear Lane Townhouses

Rear lane townhomes, with rear garages accessed from a public or private laneway, may occur within special areas of the community having higher public visibility and pedestrian activity, on public streets or on private streets. This form of housing contributes positively to the built form character and urban streetscape appearance of the neighbourhood by removing garages and driveways from the public realm and establishing a strong uninterrupted street edge that is more urban in character.

Design Guidelines:

- Rear lane townhomes may feature 2 to 3-storey building massing to provide an appropriate transition with low density residential and establish a built form scale appropriate to the planned street hierarchy. Heightened building massing at main intersections should be considered;
- Garages will be accessed from a rear laneway and may be either attached to the dwelling or detached from the dwelling. Single or double garages are permitted;
- Garages shall be complementary to the main dwelling in material, massing, character and quality;
- Front entrances shall be directly linked to the public sidewalk with a walkway; and
- Outdoor amenity areas may take the form of a functional raised terrace/balcony.



Figure 6.3i: 3-storey rear lane townhomes with wall articulation and plane variation that contributes to the character of the neighbourhood.



Figure 6.3j: Rear lane townhomes with private front yard spaces that consist of low fencing and edge planting.

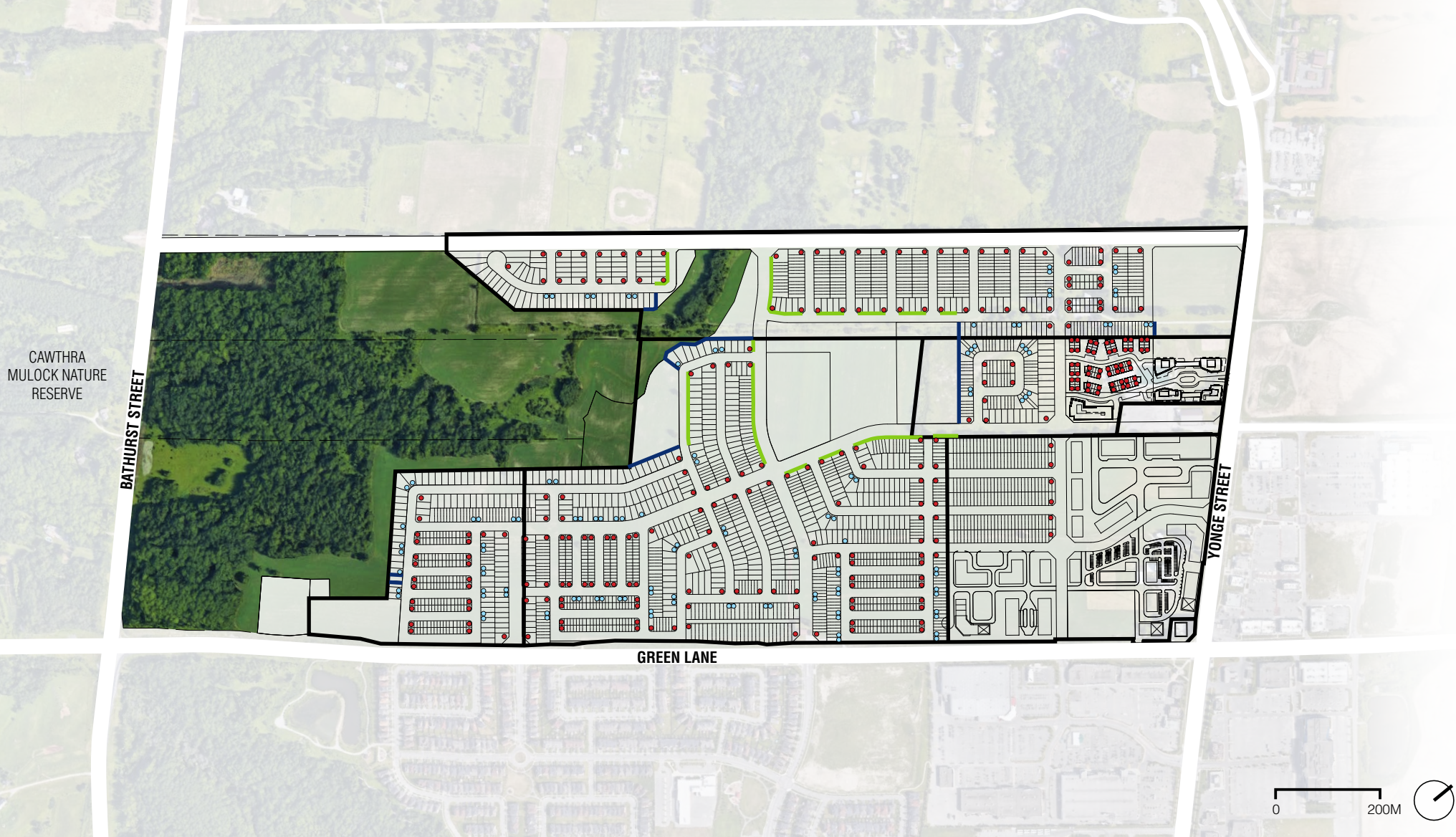


Figure 6.3k: NW Yonge Green Lane Proposed Priority Lots

6.3.4.5 Priority Lots

Priority lots are those located prominently within the community. Their visual significance within the streetscape requires that the siting, architectural design and landscape treatment of residential built form on these lots be of an exemplary quality to serve as landmarks within the community. Prominent lot locations identified have a greater degree of visibility and, therefore, require special design consideration to ensure an attractive built form, appropriate to its location, is achieved.

The priority lot plan for NW Yonge Green Lane demonstrates the lot locations requiring special design considerations, including corner lots, view terminus lots, open space facing lots, and dwellings requiring upgraded rear and side architecture. These locations shall be further assessed at the time of the subdivision level architectural control guidelines.



Figure 6.31: Single detached corner dwelling wraparound porch and main entry on the long elevation facing the flanking street.

6.3.4.6 Corner Lot Dwellings

Dwellings on corner lots typically have the highest degree of public visibility within the streetscape and are important in portraying the image, character and quality of the community.

Dwelling designs must be appropriate for corner locations, with dual façades that address both streets (e.g. porches and balconies, large windows, side entrances, etc.). Dwelling designs intended for internal lots will not be permitted unless the flankage elevation is upgraded to address the street.

- Both street frontages for corner lot dwellings and rear elevations are encouraged to reflect similar levels of architectural design and detail with respect to massing, roofline character, fenestration, materials, details, etc;
- Distinctive architectural elements, such as porches, porticos, bay windows, ample fenestration, window treatment, wall articulation, brick arrangement and colour, etc. appropriate to the architectural style of the dwelling, are encouraged on the flankage side to create an interesting streetscape and emphasize the corner dwelling's landmark function;
- The main entry of the corner dwelling is encouraged on the long elevation facing the flanking street, located at or close to the corner; and
- Driveway access on corner lots should be provided from the lower-order street.



Figure 6.3m: Townhouse end unit with distinctive corner articulation.



Figure 6.3n: Elbow street dwelling with A prominent architectural element.

6.3.4.7 View Terminus Dwellings / Street Elbow Dwellings

View terminus dwellings are situated at the top of T-intersections or street elbows, where one road terminates at a right angle to the other. These dwellings play an important role in defining a terminating long view corridor.

- A prominent architectural element, massing or material arrangement may be provided to terminate the view;
- Subject to approval of the Town's Engineering Department, driveways should be located to the outside of the lot, rather than in line with the view corridor, to reduce the impact of the garage on the terminus view and allow for front yard landscaping to become the focus, along with the architectural treatment;
- Dwellings on the outside bend of curved streets provide opportunities to create a grouping of dwellings that accentuate a special architectural and landscape theme; and
- Where dwellings step back from each other, publicly exposed side walls should be given similar architectural detailing as the front elevation.

6.3.5.4 Dwellings Requiring Upgraded Rear and Side Architecture

Upgraded rear and side architecture is required where elevations are exposed to public view, such as lots which back or flank onto roads, parks, walkways, and public open space areas.

- The exposed side and/or rear elevations of dwellings in these locations shall have a level of quality and detail that is more consistent with the front elevation of the dwelling; and
- The level of upgrading should be consistent with the level of public exposure.

6.3.4.7 Open Space Facing Dwellings

Given the prominence of the proposed SWM facilities, the central community/neighbourhood park, the two schools, and their function as a focus and gathering space for the community, dwellings that front onto these features shall be designed in a manner that considers and complements their exposure. The following guidelines shall apply to dwellings facing open space features:

- An enhanced built form treatment consistent with the architectural style shall be implemented, such as prominent front porches, pronounced, well-proportioned windows, a projecting bay, articulated wall treatment and other design elements that enhances the front elevation;
- The use of upgraded materials and detailing, such as stone or precast elements, dichromatic brick, quoining, etc. is encouraged to be integrated into the elevation design, consistent with the architectural style; and
- As zoning and architecture permits, dwellings are encouraged to feature large porches allowing for multiple seating and deeper porches to promote 'eyes on the street', which results in an informal monitoring of the park and its activities.



Figure 6.3o: Dwellings with upgraded rear architecture exposed to an open space feature.



Figure 6.3p: Park facing dwellings with enhanced built form and upgraded materials that signify their prominence in the streetscape.



Figure 6.3q: Mid-rise buildings with appropriate density transitions.

6.3.6 Mid-Rise Residential and Mixed-Use Buildings

With a mix of densities being offered in the community, the mid-rise residential and mixed-use buildings will include a range of configurations that can be attached to or share the block with high-rise buildings resulting in the juxtaposition of heights and punctuation of roofscape profiles that avoid monotony of massing along the streetscape. Blocks containing mid-rise buildings have been configured in a variety of forms to aid in stepping-down the height and scale of taller buildings, transitioning between building types and establishing the appropriate height to proportionately frame larger-scale open spaces to create a sense of enclosure. Mid-rise building forms may include mixed-use, residential, and retirement/hotel uses. Aligning with Town OP section 4.3.5.8, built form within the residential mixed-use blocks is intended to have a minimum height of 3-storeys (approximately 9 metres) and a maximum height of 12-storeys (approximately 37 metres). In addition to applicable built form policies outlined in OP section 3.2.3.2.1 *Major Local Centres*, the following mid-rise building guidelines should be considered:

Design Guidelines:

- Mid-rise building heights typically range from 3 to 12-storeys, with podiums as low as 1 storey;
- Buildings should be designed to mitigate any negative impact upon surrounding development;
- A shadow impact study may be required, depending on building height, location and orientation relative to adjacent land uses;
- Ground level floor heights should generally be taller than upper floor heights to create a strong street presence and provide opportunities for flexible space, such as retail and service uses;
- Building setbacks should be minimized to relate well to the adjacent roadway and open space areas, while allowing sufficient space for a comfortable pedestrian zone and landscape planting opportunities;

- Building façades should provide visual interest through use of materials, colours, ample fenestration, wall articulation and style appropriate architectural detailing. All façades exposed to public view should be well articulated and detailed;
- Corner buildings should provide façades which appropriately address both street frontages;
- Main entrances should be designed as a focal point of the building. They should typically be recessed or covered and provide visibility to interior lobbies to allow for safe and convenient arrival and departure from the building. Main entrances shall also be ground-related and wheelchair accessible; and
- Building materials and detailing should be used to establish a lower building and upper building:
 - Lower building - the first few storeys of a mid-rise building, including the ground floor and any additional floors with direct relationship to the street and public realm. Generally, this would include those storeys forming the streetwall and not those stepped back from the streetwall.
 - Upper building - above the lower building, designed to fit with and achieve an appropriate relationship with the lower building, the public realm, and neighbouring properties.



Figure 6.3r: Building materials and detailing are used to establish a lower and upper portion of the building.



Figure 6.3s: High-rise buildings on podiums enhance the character of active urban streets.

6.3.7 High-Rise Residential and Commercial/Mixed-Use Buildings

High-rise residential and commercial/mixed-use buildings are proposed in blocks near the edges of the community at major gateways, with the tallest buildings planned where there is an anticipated intensity of use. A high degree of architectural design quality should be exhibited to ensure a distinct and attractive built form character appropriately suited to the building's prominent location near the intersection of regional arterial roads in a Major Local Centre. Each building will be reviewed and approved by the Town through a Site Plan Approval process based in part on its design merits, its compatibility with neighbouring buildings, and its ability to appropriately fit within the built form context of the community. Final building heights shall be determined in consultation with the Town and shall comply to the zoning by-law.

As noted in the Town OP Policy 3.2.3.2.1 *Major Local Centres*, given the width and vehicular nature of both Yonge Street and Green Lane West, mixed-use shopping streets may be focused along internal roads that connect into Yonge Street or Green Lane. However, the development must address the public realm associated these arterial roads in an appropriate manner to the satisfaction of the Town.

In support of the vision for NW Yonge Green Lane, the high-rise buildings will be oriented and designed to enhance placemaking, wayfinding, and landmarking of the public realm. For this reason, these taller buildings will be uniquely configured in a variety of forms to aid in stepping-down the height and scale, transitions between building types, and establishing the form of massing and proportion of tower shafts in relation to views from streets and open spaces.

In addition to the Town OP policies for high density built form in Major Local Centres, the high-rise building guidelines in the following sections should also be considered:

6.3.7.1 Building Base (Podium)

The lower storeys of a tall building are referred to as the building's base or podium and act as an anchor to the tower elements of taller buildings. The location and height of the base element is intended to frame the public realm and has therefore been carefully designed to reinforce an attractive and interesting streetscape at a pedestrian level.

Podium Location and Site Design

- Where no street wall has been established inside the development area, the podiums will be set back at grade, to create wide boulevards that accommodate pedestrians, landscaping, and active at-grade uses;
- Clear glazing and strategic arrangement of internal building uses should be utilized to create a visual connection between the public and private areas. This connection fosters a sense of openness and transparency, promoting a harmonious integration of both realms; and
- The ground floor of podiums within mixed-use buildings should offer a mix of services such as health and wellness services, multi-purpose rooms, administrative offices, concierge, lounge areas, and publicly accessible retail, service, office, library, daycare, commercial areas, or other building amenities.



Figure 6.3t: Image examples of podium design where the use of well articulated architectural elements such as ample fenestration, canopies, sensitively placed signage and emphasized entrances combine to achieve a positive streetscape presence.

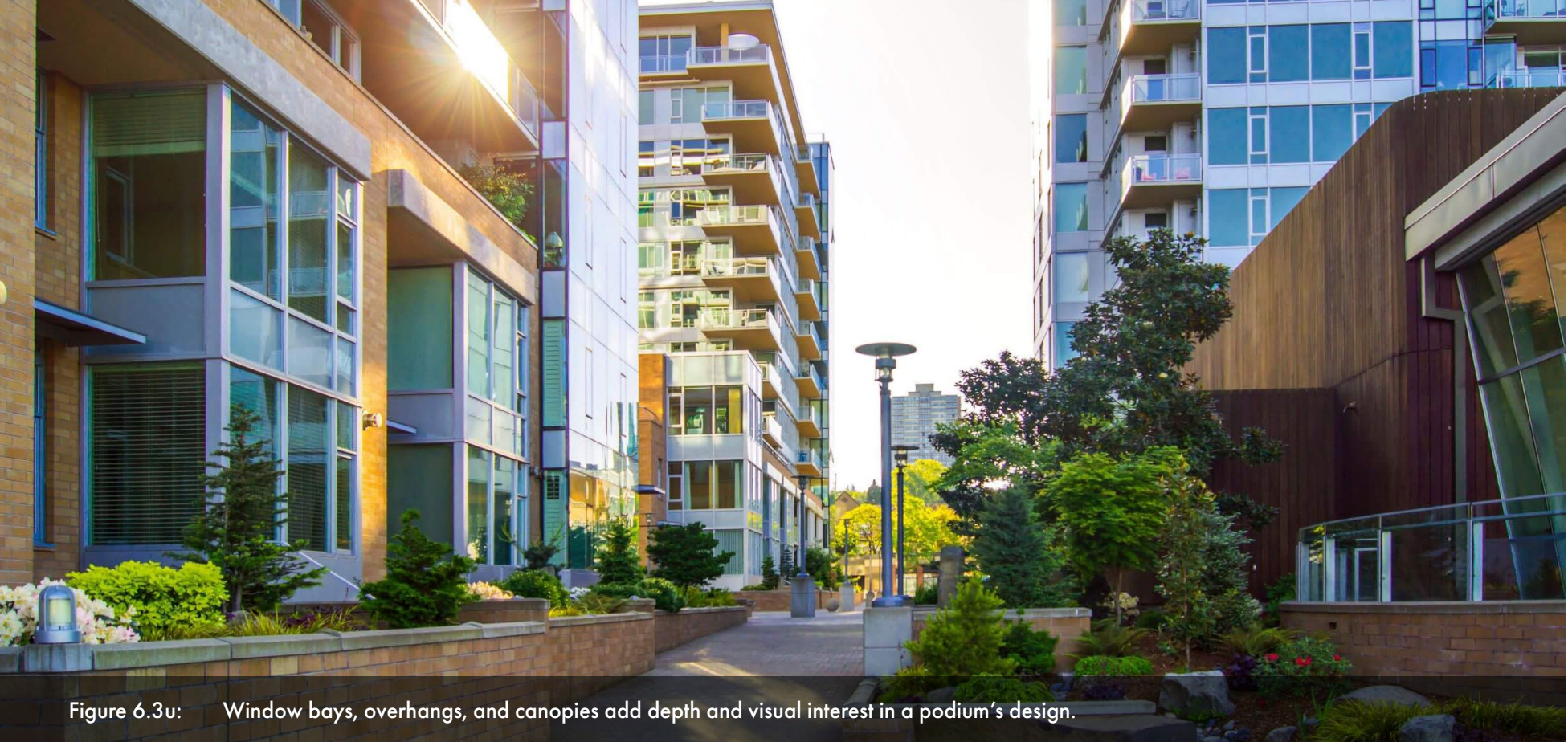


Figure 6.3u: Window bays, overhangs, and canopies add depth and visual interest in a podium's design.

Podium Design and Articulation

- The podium design should incorporate elements and materials that enhance the pedestrian experience and support a safe and active pedestrian presence within the development;
- Design the podium with clear views into the buildings and articulated entries to facilitate a safe environment for building occupants and pedestrians;
- Articulate the podium with high-quality materials and design elements that contribute to a pedestrian scale;
- Consider incorporating window bays, overhangs, and/or canopies in the podium design to add depth and visual interest;
- Provide ample fenestration in the podium design to promote natural light and a sense of openness;
- Use exterior material patterns to add texture and dimension to the podium façades. Thoughtful material selection contributes to the visual appeal of the building while harmonizing with neighboring structures; and
- All façades, including those not immediately visible from the public realm, should undergo the same design considerations to maintain architectural coherence throughout the development.

6.3.7.2 Building Middle (Tower)

The tower, or building middle, being the central and visually commanding element of a tall building, plays a pivotal role in shaping the Town's skyline. As the most substantial and visually prominent component of a tall building, the tower should be designed to enhance the skyline and provide a defining landmark for the cityscape. To achieve this goal, the following guidelines should apply:

Tower Location

- The tower element should be positioned away from main streets, parks, and neighbouring properties to minimize negative visual and physical impacts. By doing so, the podium becomes the defining element for the public realm, enhancing the overall streetscape and community experience;
- The placement of towers should be coordinated to maximize access to sunlight and sky views from the main streets, parks, and neighbouring properties; and
- Generous tower separation distances of at least 25.0m between all proposed towers should be maintained within the development. This approach ensures that tall buildings fit harmoniously within the site's context and avoids overcrowding or overshadowing of neighbouring properties.

Tower Height and Massing

- The height and massing of the tower element should facilitate an appropriate physical transition to mitigate negative impacts on adjacent established or planned land use conditions;
- The massing of tower elements and their relationship to the building base should not result in adverse wind effects at the street level. Design measures should be implemented to mitigate wind impacts and enhance pedestrian comfort; and



Figure 6.3v: Building tower positioned away from the main streets and parks/open space.

Shadows and Sky Views

- Maximum daylight access into each residential unit should be ensured, promoting a healthy and sustainable living environment.

Tower Design and Articulation

- The tower element should be designed with a focus on improving building energy performance;
- Views and vistas should enhance the overall living experience for residents;
- Visual interest in the tower design should be provided through the incorporation of balconies, façade articulation, and a thoughtful play of materials and colors. These design elements will add depth and character to the building while contributing to the overall aesthetic appeal;
- Building materials should be selected that are of high quality, durable, and sustainable. The chosen materials should contribute to the long-term integrity of the development and align with sustainable building practices;
- The architectural character of the proposed towers should be compatible with the design of the podium. The towers should harmonize with the overall development, creating a cohesive and integrated architectural expression;
- The use of material to articulate unique components of the tower façades should be considered. These materials can add visual interest and texture to the design, complementing the overall aesthetics; and
- Facade treatments associated with heavy materials should be applied closer to the podium of the buildings to avoid making the tower appear too heavy. Thoughtful distribution of materials, including brick and glass will help create a balanced and visually appealing tower design.

6.3.7.3 Building Top (Cap)

Many tall buildings will serve as an urban backdrop and frame existing landmarks or public open spaces. Under these circumstances, the design of the top of tall buildings can make a positive contribution to the character and quality of the Town skyline by supporting the overall architectural vision of the tower and podium design.

Rooftop Design and Articulation

- The top of the building should be differentiated from the tower and the building profile minimized. This can be accomplished by design measures such as incorporating stepbacks, material variations, or unique articulation in the design of upper floors;
- Tower caps visible at terminating views within the community may be conceived as distinctive landmarks, and should incorporate signature features such as energy-efficient decorative lighting, terraces, or rooftop gardens; and
- Rooftop amenity spaces should be integrated in the design, whenever feasible, to promote activity and engagement in the upper levels of buildings.

Mechanical Penthouse

- The rooftop mechanical equipment should be positioned or screened away from public view to protect and enhance views from the surrounding area and neighboring towers; and
- Mechanical penthouse will step back appropriately from the edge of the floor below, ensuring seamless integration into the built form and providing adequate screening.

6.3.8 Non-Residential Architectural Design Guidelines

6.3.8.1 Design Criteria for Schools

Schools serve as landmark buildings within the community. An elementary school has been strategically located to provide safe and logical accessibility by pedestrians, cyclists and motorists, and to achieve maximum visibility from surrounding areas, through siting at a prominent intersection and providing linkages with the open space system and trail network. In addition to Town OP Policy 3.3.3 *Urban Design Policies for Institutional, Commercial and Industrial Development*, the following guidelines shall be considered.

Design Guidelines:

- School buildings located on corner sites should be situated close to the intersection and address both street frontages in a consistent manner. Main entrances shall be directly visible from the street and be given design emphasis.
- A strong built form relationship to the surrounding streets should be created through minimum building set-backs and direct access to the main entry from adjacent sidewalks.
- Each school may develop its own distinct visual identity, while harmoniously blending into the community fabric. Architectural styles, materials and colours should relate to the character envisioned for the surrounding community. Quality building materials shall be used, including brick or stone as the main wall materials.
- Schools shall incorporate prominent building features into their design, which will help to reinforce their landmark function within the community.
- 2-3-storey building massing shall be provided.



Figure 6.3w: School uses complementary to park features.

- Buildings shall be located to ensure good sight lines for all vehicular access points and to create coherent on-site traffic circulation. Vehicle circulation at the front of the school shall, typically, be limited to drop off zones.
- Minimize the impact of main parking facilities from the street edge through siting (at the rear or side of buildings away from the street) and landscape buffer treatment.
- Conflicts between pedestrian and vehicular routes shall be avoided. Adequate setback between building entrances and on-site traffic routes should be provided. Pedestrian routes should be well defined and provide easy, direct and barrier-free access to school entrances.

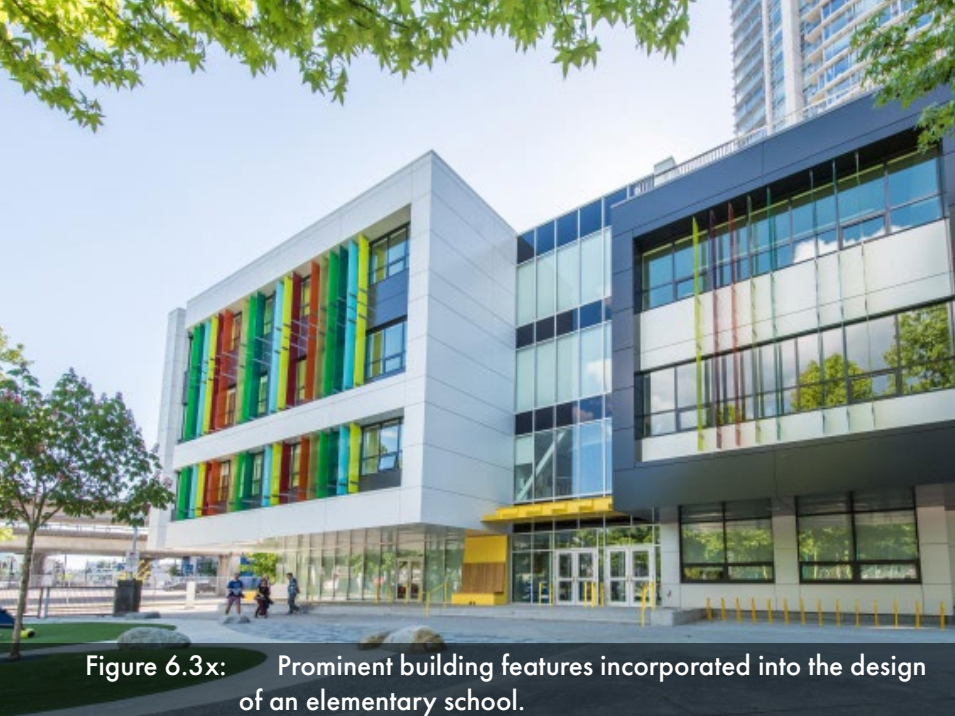


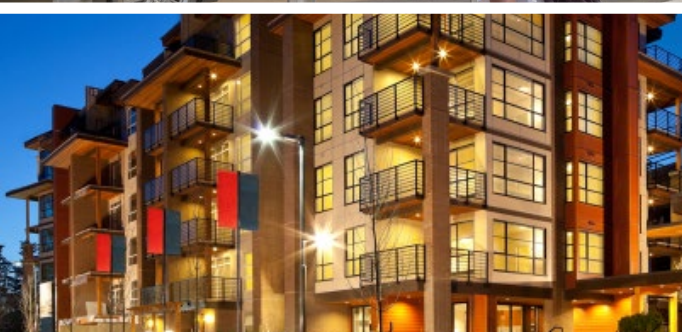
Figure 6.3x: Prominent building features incorporated into the design of an elementary school.



Figure 6.3y: A school with a strong built form relationship to the surrounding streets.

- Parking areas, driveways and walkways shall be adequately illuminated with low level, pedestrian scaled lighting.
- Paved surfaces on school sites shall be provided in accordance with the applicable School Board requirements for parking and barrier-free play areas.
- Lighting for school buildings shall be integrated into the architecture. Lighting shall be directed downward and inward to avoid light spill-over onto adjacent properties.
- Signage shall be incorporated into the building architecture. Where ground level signage is used it shall be designed as a landscape feature, integrating other components such as planting, lighting, etc.
- Loading, service and garbage areas shall be integrated into the building design or located away from public view and screened to minimize negative impacts.
- Utility meters, transformers and HVAC equipment shall be located away from prominent public views.
- Rooftop mechanical equipment shall be screened from ground level view by integrating into the roof or a parapet.

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conclusion

7

Reflecting the vision established for the Green Lane Secondary Plan Area, the proposed development demonstrates an appropriate plan for the subject lands that aligns with the planning policy direction and urban design goals of the Town of East Gwillimbury. It is designed to be an integral part of the growing Region and Town, developed to fit seamlessly within the framework provided by the Official Plan and surrounding land uses.

The CDP establishes an urban built form that is planned and oriented to people, and creates an active and attractive urban edge along the surrounding established and future community. It also provides efficient connections to public streets, sidewalks, and nearby multi-use trails, reinforcing active transportation opportunities that will result in a more accessible and sustainable environment for all.

This document has addressed design considerations such as the streetscape, open space, proposed urban structure, and built form, providing design guidance as the basis for development while the detailed plan for each phase evolves. Detailed design will be implemented through the final development design and Site Plan Approval process, adhering to the Town's OP policies for the Green Lane Secondary Plan.

A residential architectural control process will be implemented for the development of Northwest Yonge Green Lane. This process involves the preparation of an architectural control guidelines document and an overview of the architectural control implementation process. The assessment will encompass the Control Architect's review and approval of residential dwelling siting and compliance with the conditions prescribed in the design guidelines.



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